



# Community Planning & Permitting

Courthouse Annex • 2045 13th Street • Boulder, Colorado 80302 • Tel: 303-441-3930  
Mailing Address: P.O. Box 471 • Boulder, Colorado 80306 • [www.BoulderCounty.org](http://www.BoulderCounty.org)

## PUBLIC MEETING

### BOULDER COUNTY, COLORADO HISTORIC PRESERVATION ADVISORY BOARD

**DATE:** Thursday, May 5, 2022  
**TIME:** 6:00 pm

**PLEASE NOTE:** Due to COVID-19 concerns, this hearing will be held virtually. Information regarding how to participate will be available on the Historic Preservation Advisory Board webpage in advance of the hearing ([www.boco.org/HPAB](http://www.boco.org/HPAB)).

Notice is hereby given that a Public Hearing will be held by the Boulder County Historic Preservation Advisory Board (HPAB) at 6:00 pm to consider the following agenda:

1. Citizen participation for items not otherwise on the agenda
2. Approval of minutes from previous meetings
3. Building Permit Reviews for Structures 50 Years of Age and Older
4. Other Business
  - a. Discussion on the History and Future of the Pillars at Nine Mile Corner (Hwy 287 & Arapahoe Rd) Presentation by William Meyer & soliciting input from HPAB

Contact the Historic Preservation Team at [historic@bouldercounty.org](mailto:historic@bouldercounty.org) for more information on these items. Please note that the HPAB agenda is subject to change.

*It is Boulder County policy to make county programs, meetings, activities, and services accessible to individuals with disabilities, according to the Americans with Disabilities Act. If you need special assistance, contact the ADA Coordinator at 303-441-3525 at least 72 hours before the scheduled meeting.*

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Published: April 28, 2022 -- Longmont Daily Times-Call

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Apr 28, 2022

  
Signature

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28th day of April, 2022

  
Notary Public

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**MEETING OF THE HISTORIC PRESERVATION ADVISORY BOARD  
BOULDER COUNTY, COLORADO  
AGENDA**

**Thursday, May 5, 2022, 6:00 p.m.**

**Virtual HPAB Meeting**

Due to COVID-19 concerns, this hearing will be held virtually. Information regarding how to participate will be available on the Historic Preservation Advisory Board webpage approximately one week prior to the hearing at [www.boco.org/HPAB](http://www.boco.org/HPAB). To join the meeting by phone, dial 1-833-568-8864 (toll free) and enter the Meeting ID: 161 799 1328.

This agenda is subject to change. Please call ahead (303-441-3930) or check the Historic Preservation Advisory Board webpage to confirm an item of interest. For special assistance, contact our ADA Coordinator (303-441-3525) at least 72 hours in advance.

There will be opportunity to provide public comment remotely on the subject items during the respective virtual Public Hearing portion for each item. If you have comments regarding any of these items, you may mail comments to the Community Planning & Permitting Department (PO Box 471, Boulder, CO 80306) or email to [historic@bouldercounty.org](mailto:historic@bouldercounty.org). Please reference the docket number of the subject item in your communication. Call 303-441-3930 or email [historic@bouldercounty.org](mailto:historic@bouldercounty.org) for more information

- 
1. **CALL TO ORDER**
  2. **ROLL CALL**
  3. **CITIZEN PARTICIPATION FOR ITEMS NOT OTHERWISE ON THE AGENDA**
  4. **APPROVAL OF MINUTES**
    - 4.1. Approval of Meeting Minutes for April 7, 2022
  5. **BUILDING PERMIT REVIEWS FOR STRUCTURES 50 YEARS OF AGE AND OLDER**
  6. **OTHER BUSINESS**
    - 6.1. Discussion on the History and Future of the Pillars at Nine Mile Corner (Hwy 287 & Arapahoe Rd)  
Presentation by William Meyer & soliciting input from HPAB
  7. **ADJOURNMENT**



## MEETING OF THE HISTORIC PRESERVATION ADVISORY BOARD BOULDER COUNTY, COLORADO

### Regular Meeting Minutes

April 7, 2022, 6:00 p.m.  
Virtual HPAB Meeting

Board Members Present: Chuck Gray

Don Burd  
Elizabeth Gehring  
Larry Powers  
Marissa Ferreira  
Mark Gerwing  
Jason Emery  
Terry Walters  
David Hawes

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#### 1. **CALL TO ORDER**

The meeting was called to order at approximately 6:01 p.m. by Chair Chuck Gray.

#### 2. **ROLL CALL**

#### 3. **CITIZEN PARTICIPATION FOR ITEMS NOT OTHERWISE ON THE AGENDA**

None

#### 4. **APPROVAL OF MINUTES**

##### 4.1 **Approval of Meeting Minutes for February 3, 2022**

**MOTION:** Jason Emery MOVED that the Boulder County Historic Preservation Advisory Board APPROVE the meeting minutes from February 3, 2022.

**SECOND:** Larry Powers

**VOTE:** Motion PASSED {9:0}

5. **BUILDING PERMIT REVIEWS FOR STRUCTURES 50 YEARS OF AGE AND OLDER**

None

6. **LANDMARKS**

6.1 **Docket HP-22-0002: Windwhistle**

Denise Grimm, Principal Planner, gave the staff presentation. An application for landmark designation of Windwhistle has been submitted by the owners, Street Cowbell Hill Cabin Trust. The application is for both the cabin and the bunkhouse on an approximately .48-acre parcel next to Allenspark. A log woodshed on the property is not included in the landmark application.

The cabin dates to c. 1924 and was built for Allen and Erma Street. The Streets were from Oklahoma City and first vacationed in Allenspark at the invitation of notable resident Frank Gay. Allen Street was prominent in Oklahoma City as both a member of the State House of Representatives and as mayor.

In the late 1960s, Gordon Street, the son of Allen and Erma, retired to Allenspark with his wife Nell Phillips and took ownership of the cabin in 1974. They were active community members. In 2007, the cabin transferred into a trust and is currently owned by the five children of Gordon and Nell Phillips, just short of 100 years of ownership by the same family. Daughter, Sue Nell Phillips, has captured some of the family's Colorado experience in her book *Out of Red Dirt (And Up Cowbell Hill)*.

The cabin was constructed of fire-hardened logs from an 1894 forest fire. The application notes, "The cabin also has unique chinking, a cement underlayer covered with strips of lodgepole pine." The cabin has a large stone fireplace and a few of the original windows have been replaced. The bunkhouse is also built of logs and has had very few alterations.

**SIGNIFICANCE**

The Architectural Inventory Form notes that the cabin is eligible for local landmark status under Criteria 1, 4 and 8, and hints at Criterion 3. The landmark application adds Criterion 3.

Criterion 15-501(A)(1) The character, interest, or value of the proposed landmark as part of the development, heritage, or cultural characteristics of the county;

The cabin and bunkhouse are significant for their association with the development of Allenspark as a rustic tourist resort.

Criterion 15-501(A)(3) The identification of the proposed landmark with a person or persons significantly contributing to the local, county, state, or national history;

The property is significant for its association with the Allen and Erma Street family who contributed to the development of Allenspark and have owned the property for nearly 100 years.

Criterion 15-501(A)(4) The proposed landmark is an embodiment of the distinguishing characteristics of an architectural style valuable for the study of a

period, type, method of construction, or the use of indigenous materials;

The cabin is significant for its Rustic architectural style of fire-hardened logs from a local forest fire.

Criterion 15-501(A)(8) The relationship of the proposed landmark to other distinctive structures, districts, or sites which would also be determined to be of historic significance;

The cabin and bunkhouse would likely be a contributing feature to an Allenspark historic district if such a district were created.

#### **RECOMMENDATION**

Staff recommends that the Historic Preservation Advisory Board APPROVE and recommend that the BOCC approve Docket HP-22-0002: Windwhistle under Criteria 1, 3, 4 and 8 and subject to the following conditions:

1. Alteration of any exterior feature of the structures will require review and approval of a Certificate of Appropriateness (CA) by Boulder County (note: applicable county review processes, including but not limited to Site Plan Review, may be required).
2. Regular maintenance which prolongs the life of the landmark, using original materials or materials that replicate the original materials, will not require review for a Certificate of Appropriateness, provided the Community Planning & Permitting Director has determined that the repair is minor in nature and will not damage any existing features. Emergency repairs, which are temporary in nature, will not require review (note: Depending on the type of work, a building permit may still be required.)

The applicant, Sue Nell Phillips, a trustee for Street Cowbell Hill Cabin Trust (owner), commented on the proposal and was available for questions.

#### **OPEN PUBLIC COMMENT**

None

#### **CLOSE PUBLIC COMMENT**

**MOTION:** Mark Gerwing MOVED that the Boulder County Historic Preservation Advisory Board APPROVE and recommend that the Board of County Commissioners APPROVE Docket HP-22-0002: Windwhistle for landmark status under Criteria 1, 3, 4, and 8 and subject to the two standard conditions in the Staff Recommendation.

**SECOND:** Larry Powers

**VOTE:** Motion PASSED {9:0}

#### 6.2 Docket HP-22-0003: Murray-Culver Farm

Denise Grimm, Principal Planner, gave the staff presentation.

An application for landmark designation of the site has been submitted by Boulder County Parks and Open Space. The landmark site is an approximately .47-acre area of a 28.94-acre parcel. The site includes four (4) contributing resources which are the house, the barn, the shed, and the chicken house; and one (1) non-contributing structure which is the shop/garage.

The property changed owners several times and does not have significance related to a specific person. Thus, the name Murray-Culver Farm has been chosen to represent its origins as a farm.

The property is associated with the late 19th and early 20th century development of agriculture in the Hygiene area and represents a family farm property type with a house and assortment of agricultural outbuildings. This property type, once prolific throughout Boulder County, has rapidly disappeared throughout the county because of increasing development pressures and decreasing farming activities.

The property retains the key farm buildings of the house, barn, shed, and chicken house. Viewed together, the farm buildings are historically significant as an intact example of family farm property type that retains sufficient historic physical integrity from their periods of significance with regard to their location, setting, materials, workmanship, feeling, association, and design.

Minor repairs and moderate alterations have been made to the farm's outbuildings and were largely done simply and with like materials. The house has a moderate degree of alterations with small south addition that is over 50 years of age, undated front porch changes that removed the four wood column roof supports and half knee wall, and enlargement of the front door opening to accommodate a sliding glass patio door. The moderate alterations to the house and outbuildings do not have a large enough visual impact to diminish the site's overall character and ability to convey its historic significance.

On January 16, 2020, a subcommittee of the HPAB unanimously agreed (3-0) that the property is eligible for landmark status under Criterion 1, and the barn is eligible under Criterion 4. They then agreed that the site should be landmarked as a condition of approval for docket LU-19-0042. The Limited Impact Special Use Review is to allow a Forestry Processing and Sort Yard to the west and southwest of the proposed landmark site. The subcommittee also asked for screening with shrubbery between the landmark site and the new development.

On February 9, 2021, a subcommittee of the HPAB re-reviewed the property and made the same determination.

### **SIGNIFICANCE**

The property qualifies for landmark designation under Criteria 1 and 4.

Criterion 15-501(A)(1) The character, interest, or value of the proposed landmark as part of the development, heritage, or cultural characteristics of the county;

The property is significant for its association with the development of early agriculture in Boulder County.

Criterion 15-501(A)(4) The proposed landmark is an embodiment of the distinguishing characteristics of an architectural style valuable for the study of a period, type, method of construction, or the use of indigenous materials;

The property is significant because the barn is an excellent example of a front gabled single wing barn.

#### **RECOMMENDATION**

Staff recommends that the Historic Preservation Advisory Board APPROVE and recommend that the BOCC approve Docket HP-22-0003: Murray-Culver Farm under Criteria 1 and 4 and subject to the following conditions:

1. Alteration of any exterior feature of the structures or construction within the site area will require review and approval of a Certificate of Appropriateness (CA) by Boulder County (note: applicable county review processes, including but not limited to Site Plan Review, may be required).
2. Regular maintenance which prolongs the life of the landmark, using original materials or materials that replicate the original materials, will not require review for a Certificate of Appropriateness, provided the Community Planning & Permitting Director has determined that the repair is minor in nature and will not damage any existing features. Emergency repairs, which are temporary in nature, will not require review (note: Depending on the type of work, a building permit may still be required.)

Carol Beam, Cultural Resource Specialist with Boulder County Parks and Open Space, commented on the proposal and was available for questions.

#### **OPEN PUBLIC COMMENT**

None

#### **CLOSE PUBLIC COMMENT**

**MOTION:** Mark Gerwing MOVED that HPAB APPROVE and recommend that the Board of County Commissioners APPROVE Docket HP-22-0003: Murray-Culver Farm for landmark status under Criteria 1 and 4, subject to the two standard conditions in the Staff Recommendation, and that the proposed landmark boundary as indicated in the staff recommendation be extended by ten feet to the north, south, east, and west.

**SECOND:** Marissa Ferreira

**VOTE:** Motion PASSED {9:0}

## **7. OTHER BUSINESS**

### **7.2 Election of Officers**



The board moved forward with nominations for officers on the Historic Preservation Advisory Board.

**MOTION:** Marissa Ferreira **MOVED** that the current Chair, Chuck Gray, continue as Chair, and that the current Vice-Chair, Mark Gerwing, continue as Vice-Chair.

**SECOND:** Larry Powers

**VOTE:** Motion **PASSED {9:0}**

7.1 National Register of Historic Places Nomination of the Rock Creek Farm Rural Historic District

At approximately 6:59 p.m., board member Don Burd recused himself and left the meeting due to his involvement in the property's nomination.

Denise Grimm, Principal Planner, gave the staff presentation. As a Certified Local Government (CLG), Boulder County has the opportunity to participate in the National Register process. The State Office of Archaeology and Historic Preservation (OAHP) sends the county copies of any applications within its jurisdiction to review. Boulder County Historic Preservation Advisory Board reviewed documents for the nomination of the Rock Creek Farm Rural Historic District. Boulder County owns the property and has submitted the application.

Carol Beam, Cultural Resource Specialist with Boulder County Parks and Open Space, commented on the proposal and was available for questions.

**RECOMMENDATION**

Staff recommends that HPAB respond to the state in support of the nomination recommending that the property meets criteria A, C and D.

A) Property is associated with events that have made a significant contribution to the broad patterns of our history.

C) Property embodies the distinctive characteristics of a type, period, or method of construction or represents the work of a master, or possesses high artistic values, or represents a significant and distinguishable entity whose components lack individual distinction.

D) Property has yielded, or is likely to yield, information important in prehistory or history.

**MOTION:** Marissa Ferreira **MOVED** to support the nomination of the Rock Creek Farm Rural Historic District to the National Register of Historic Places as written under the three proposed criteria.

**SECOND:** David Hawes

**VOTE:** Motion **PASSED {8:0}**

8. ADJOURNMENT

Denise Grimm discussed the upcoming awards ceremony in honor of Archeology and Historic Preservation Month taking place on May 9. Marissa Ferreira volunteered to attend the ceremony and present newly approved landmarks.

Staff and board members welcomed and introduced themselves to new member David Hawes.

The Boulder County Historic Preservation Advisory Board meeting was adjourned at approximately 7:22 p.m.



**MEETING OF THE HISTORIC PRESERVATION ADVISORY BOARD BOULDER COUNTY, COLORADO**

**Regular Meeting Minutes**

**May 5, 2022, 6:00 p.m.  
Virtual HPAB Meeting**

Board Members Present: Mark Gerwing  
Jason Emery  
Marissa Ferreira  
Elizabeth Gehring  
David Hawes  
Larry Powers

Board Members Chuck Gray

Excused:  
Don Burd  
Terry Walters

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**1. CALL TO ORDER**

The meeting was called to order at approximately 6:05 p.m. by Vice-Chair Mark Gerwing.

**2. ROLL CALL**

**3. CITIZEN PARTICIPATION FOR ITEMS NOT OTHERWISE ON THE AGENDA**

None

**5. BUILDING PERMIT REVIEWS FOR STRUCTURES 50 YEARS OF AGE AND OLDER**

None

**6. OTHER BUSINESS**

6.1 Discussion on the History and Future of the Pillars at Nine Mile Corner (Hwy 287 & Arapahoe Rd)

Bill Meyer of the Rotary Club gave a presentation on the pillars at Nine Mile Corner located at the intersection of Arapahoe Road and Highway 287. The pillars were constructed within public right-of-way prior to its administration by the Colorado Department of Transportation (CDOT). Recent decades have seen major boosts in traffic levels, and corresponding efforts to widen the roadway. CDOT does not have a plan for preservation of the pillars. The Board discussed different options available to repair and preserve the pillars and possible alternative locations for the pillars should they need to be moved, and expressed their desire to work more with CDOT, the City of Lafayette, and others on this issue. HPAB members Jason Emery, Liz Gehring, and Larry Powers volunteered to participate in further discussions regarding the pillars.

David Hawes joined the meeting at 6:09 pm.

Elizabeth Gehring joined the meeting at 7:09 pm.

4. APPROVAL OF MINUTES

4.1 Approval of Meeting Minutes for April 7, 2022

**MOTION: Marissa Ferreira MOVED that the Boulder County Historic Preservation Advisory Board APPROVE the meeting minutes from April 7, 2022.**

**SECOND: Larry Powers**

**VOTE: Motion PASSED {6:0}**

7. ADJOURNMENT

Denise Grimm discussed the agenda for the upcoming awards ceremony in honor of Archaeology and Historic Preservation Month taking place on May 9 at Chautauqua Community House.

The Boulder County Historic Preservation Advisory Board meeting was adjourned at approximately 7:47 p.m.



## Community Planning & Permitting

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### MEMO

#### **Discussion on the History and Future of the Pillars at Nine Mile Corner (Hwy 287 & Arapahoe Rd) Presentation by William Meyer & soliciting input from HPAB**

William Meyer with the Rotary Club will present information he and others have compiled related to the history of the Pillars at Nine Mile Corner and the Road of Remembrance.

After becoming concerned about the fate of the pillars, given the recent intersection changes at that corner, Bill has been working to coordinate an effort to determine the future of the pillars and whether moving them might be an option.

I'm attaching documentation on the pillars, some of the possible alternatives that are being evaluated (though other options might also be available) and some of the comments received at some community meetings where this information has been presented.



**COLORADO**  
**Department of Transportation**  
Region 4

Director's Office  
10601 10<sup>th</sup> Street  
Greeley, CO 80634

November 18, 2021

City of Boulder Landmarks Board  
2045 13th Street  
Boulder, CO 80302

MAR 07 2022

RECEIVED  
Boulder County Public Works

Re: After the Fact Post-Project Determinations of State Register Act Eligibility and Effect  
Determinations for Intersection Improvements at State Highway 7 (Arapahoe Road) and U.S.  
Highway 287 in Boulder County

Dear City of Boulder Landmarks Board:

The Colorado Department of Transportation (CDOT) invites your comments regarding the above-referenced intersection improvement project in Boulder County, Colorado, in particular the project's impacts upon historical resources. This letter and the enclosed attachments are being sent concurrently to the Colorado State Historic Preservation Officer (SHPO) as well as a number of organizations that have an interest in historic preservation in Boulder County.

We are reaching out to you as part of CDOT's post-project or "after the fact" consultation with your office regarding a recently completed developer-driven intersection improvement project in unincorporated Boulder County, which necessitated temporary construction access within the CDOT-owned right-of-way (ROW) for Arapahoe Road/State Highway 7 (Arapahoe Road). While not a CDOT designed or constructed project, CDOT was involved by issuing a temporary access permit to the developer. Due to an oversight by CDOT Region 4, the presence of a previously recorded historical resource in the project area was overlooked, and the permit was issued. The intersection project was completed in Fall 2021. In order to achieve compliance with the Colorado State Register Act, CDOT is seeking your concurrence on after-the-fact determinations of State Register of Historic Places-eligibility for all historical resources (generally ≥50 year old), and effects to these resources from the intersection project. Since CDOT's only involvement in this project was to issue an access permit to the developers, there was no federal nexus that would have triggered the need for compliance with Section 106 of the National Historic Preservation Act (NHPA).

Background

In early July 2021, the (west side of) US 287/Arapahoe (SH 7) intersection project was brought to the attention of the CDOT Region 4 Senior Historian by a private citizen and Boulder Rotary Club member, William D. Meyer, who along with a small cadre of club members formed the Nine Mile Corner Monument Working Group (Working Group). This group is and has been concerned about the condition and preservation of the stone pillars that flank Arapahoe Road on the west side of the US 287. They were conceived as memorial monuments to American dead in the "Great War" (World War I). Constructed of sandstone in 1928 by the Boulder Lions Club, these intricate structures were major features of an elaborate gateway to a proposed - but never constructed - "Road of Remembrance" leading to Boulder. A detailed history of the pillars is presented in the attached document prepared by Mr. Meyer of the Working Group.

The 93-year-old gateway pillars have faced threats to their preservation since the early 1980s. The first threat came in 1983, when the Colorado Highway Department improved and widened Arapahoe Road at and near the intersection with and the west side of US 287. The project included the relocation of the northern gateway pillar forty feet to the north, and a specialized contractor was employed to move the structure intact. Unfortunately, the effort was unsuccessful due to unstable sandy soil which caused the pillar to tumble to the ground and break apart. The Highway Department, with specialized

contractor help, faithfully reconstructed the pillar in the new location, forty feet north of its original location. The southern pillar was left in place at its original (1928) location.

In the summer of 2017, the Evergreen Development Company was pursuing construction of a large commercial development on the southwest side of the intersection. In order to accommodate a new right turn lane from eastbound SH 7 to US southbound 287, Evergreen sought approval from CDOT to relocate the south pillar out of CDOT ROW for Arapahoe Road/SH 7 and onto the developer's parcel. The CDOT Region 4 Senior Historian advised CDOT's Permits unit that, as a significant historic resource, CDOT could not allow relocating the pillar outside CDOT ROW and incorporated into a private development. The pillars' locations comprise an especially important aspect of integrity, because they were deliberately sited in 1928 to prominently define the entrance to Arapahoe Road, which was being promoted as an important route to Boulder.

Faced with the permit denial, the developers and their design consultants altered the intersection improvement project to allow for preservation in place of the southern pillar, by surrounding it with a raised and landscaped "pork chop" island (equipped with guardrail) located between a new, separated right turn lane, and Arapahoe Road.

In the recent past, CDOT's Boulder Engineer Residency discussed the idea of potentially relocating both pillars out of the Arapahoe Road ROW and place them at a future nearby planned Bus Rapid Transit (BRT) stop on Arapahoe to in order create a "sense of place" for them; however, that idea ultimately was abandoned.

#### Project Description

This developer-driven project involved improvement of the west side intersection of Arapahoe Road and US 287 in Boulder County, to provide new curved, single-lane right-in and right-out turn lanes from Arapahoe Road onto US 287 and vice versa, that would accommodate an anticipated large increase in traffic. In order to preserve the southern pillar, as required by CDOT, a "pork chop" island was built around it, separating the SH 7 roadway and the new right turn lane onto southbound US 287. The reconstructed northern pillar was not touched by the project, and has been protected from damage by a chain link fence (see enclosed 2021 inventory form prepared by CDOT). This fence was likely installed by the contractor involved in constructing the intersection improvements. Plans prepared for the recently completed intersection improvement project are enclosed for your reference.

#### Determinations of Eligibility and Effects

##### **5BL.432 - Arapahoe Road gateway pillars:**

This resource consists of two (north and south) elaborately designed and built mortared stone masonry towers or pillars flanking Arapahoe Road/SH 7 near its intersection with the west side of US 287. The pillars were envisioned and constructed as memorials to World War I American war dead, and as part of an elaborate gateway to a proposed "Road of Remembrance." The pillars are approximately 20 feet tall and are constructed of locally-quarried sandstone to intentionally replicate the appearance and general character of architecture on the University of Colorado in Boulder. Both structures are identical in design, and each has a small door leading to a small internal chamber. A cornerstone is placed at the base of the south pillar (only), while the north pillar bears a bronze dedicatory plaque. Detailed descriptions and photographs of the pillars are presented in the enclosed inventory form, and their history is explained in great detail in the enclosed document prepared by William Meyer.

Eligibility: The pillars were originally inventoried as the "Road to Remembrance Gateway" in May 1980 by Manuel M. Weiss of the Boulder County Historical Society, as part of the Boulder County Historical Site Survey, Boulder and Broomfield Counties (Report No. MC.LG.R20). Weiss prepared a Colorado Historical Society Inventory Record, and evaluated the gateway pillars as eligible for the National Register of Historic Places (NRHP). The resource has a "field eligible" determination date of May 1, 1980.

CDOT revisited, rerecorded and reevaluated the site in the summer of 2021. The reevaluation effort benefited immensely from the extensive research and detailed narrative report by the Nine Mile Corner Monument Working Group of the Boulder Rotary Club, including Mr. William Meyer, about the history of what was historically known as Nine Mile Corner (the US 287 and Arapahoe Road intersection), including the gateway pillars. They are the only remnants of the 1928 war memorial gateway that formerly included a small park with a captured German artillery piece and flagpole. Based upon an abundance of information, CDOT finds that the memorial pillars meet the eligibility criteria of the State Register of Historic Places. The site is historically significant under Criterion A for its association with a widespread movement in the 1920s led by fraternal organizations and philanthropic clubs to install public memorials to honor fallen American soldiers from the First World War. Under Criterion C, the pillars are significant for their intricate, collegiate architecture-inspired design and elaborate stonework construction.

Effect: The integrity of the pillars (5BL.432) has been greatly impacted by both the 1983 and 2021 intersection improvement projects. The 1983 Highway Department project resulted in the accidental destruction of the north pillar, and while the rebuilt structure retains the same characteristics and appearance as the intact southern pillar, it is located 40 feet north of its original location, but still closely flanking a widened Arapahoe road.

The 2021 intersection project left the southern pillar in its original location, but constructed a raised “pork chop” traffic island around it. Instead of flanking the highway, the pillar now sits between the roadway of Arapahoe Road and a new curving single-lane right turn lane that that routes traffic behind the pillar - essentially destroying the spatial relationship of the south pillar and roadway. With the new improvements the south pillar no longer appears to be part of a gateway that westbound traffic on Arapahoe Road would pass through. The visual impact of these changes is severe, as the pillar no longer visually reveals its historic gateway function, and instead looks oddly out of place on the traffic island (with guardrail added).

The integrity of the pillars has also been impacted by the 40 ft relocation of the north pillar in 1983. Due to severe loss of integrity of the historic setting, CDOT has determined that the developer-driven Arapahoe Road and US 287 intersection improvement project has resulted in an *adverse effect* to 5BL.432, the Arapahoe Road gateway pillars.

#### **5BL.11936 - State Highway 7 in Boulder County (5BL.11936)**

Eligibility: Based upon the 2016 CDOT-sponsored Colorado Historic Highways Inventory by consultant Mead & Hunt, State Highway 7 was determined to be officially eligible for inclusion in the NRHP.

Effect: The project involves the Arapahoe Road/US 287 west side intersection which in the late 1920s was improved in conjunction with plans to promote Arapahoe Road as a major route to Boulder and draw more traffic to the city. The entrance to this route was designed to capture the attention of motorists and serve as a World War I memorial. Imposing tall stone pillars were erected flanking the road, and a tiny park with a WW I German trophy cannon and a flagpole was established in the space between the “wye” design of the intersection. The intersection improvements provided for two, two-way sweeping curves that funneled westbound traffic from the Lincoln Highway/US 287 past the flag and cannon, and through the narrow gap between the pillars. For eastbound traffic on Arapahoe Road, the pillars appeared as a gate on the horizon, with a cannon and flagpole strikingly centered between them. As the eastbound drivers passed between the pillars, they took one of the sweeping curves right or left to the Lincoln Highway.

Subsequently, in 1983, the Colorado Highway Department (now CDOT) redesigned and constructed the intersection to make it safer and accommodate a higher volume of traffic using both Arapahoe Road and US 287. At that time the road was widened to the north, requiring relocation of the northern gateway pillar. New turn lanes were constructed, the small war memorial park was removed, and the overall cross-section of Arapahoe Road of a short stretch of Arapahoe was widened and upgraded. The recently completed 2021 developer-driven project again redesigned the intersection, removing the 1983 intersection improvements and constructing new dedicated right-in and right-out turn lanes. The new



eastbound Arapahoe Road to southbound US 287 turn lane required the use of more land on the south side of Arapahoe within CDOT-owned ROW for Arapahoe Road. In order to preserve the south pillar within CDOT-owned ROW for Arapahoe Road, it was left in place but surrounded by a raised irregular-shaped traffic island, and the eastbound turn lane now passes behind the pillar - resulting in a major, but localized change to Arapahoe Road. The 2021 configuration is markedly different than the original 1928 design. Westbound traffic now is routed between the pillars, not on a sweeping arc but instead on a single straight lane of multi-lane highway. The curves and park are long gone, and the pillars are spread nearly twice as far apart as the original designers intended.

For eastbound traffic on Arapahoe Road, the right turn lane built in 1983 has been converted to an additional thru lane, with both thru lanes passing between the widened space between the pillars. A third lane - a new right turn lane - has been added to the south of the south pillar, so that this pillar now appears to approaching eastbound traffic as directly in the middle of Arapahoe Road.

Both the 1983 and 2021 intersection improvement projects substantially altered the alignment and width of Arapahoe Road at the intersection and extending west for approximately 510 feet (~1/10 mile) to accommodate the new turn lanes. A taper from the pre-existing roadway extends approximately 160 feet eastward from the project's western terminus, and the footprint gradually widens as the new right turn lane diverges away from the road's main thru lanes. The redesigned and constructed intersection does not bear any resemblance to the 1928 design. While the 1928 intersection has been severely altered and has lost most aspects of its historic integrity, the changes involve a very short portion of Arapahoe Road and does not substantially reduce the integrity of the entire route of Arapahoe Road/SH 7. Therefore, CDOT finds that the 2021 project resulted in a finding of *no adverse effect* with respect to this linear historic resource.

#### Proposed Mitigation

In order to mitigate the adverse effect to 5BL.432, the Arapahoe Road gateway pillars, CDOT proposes to engage with a number of stakeholders/interested parties and historic preservation agencies to cooperatively develop a plan for their preservation, monitoring and perpetual maintenance. Public access and interpretation will also be considered. CDOT will invite consulting parties to participate in a Mitigation Working Group. The Mitigation Working Group will hold meetings to discuss and develop a historic preservation plan based on the analysis of all possible preservation alternatives including the potential relocation of one or both of the pillars. CDOT will manage this effort.

Mitigation Working Group members would include the Boulder Rotary Club's Nine Mile Corner Monument Working Group, including Mr. William Meyer, who first brought this situation to our attention. Other invitees include governmental and non-governmental organizations, including the Boulder County Commissioners, the Boulder County Historic Preservation Advisory Board, the Lafayette Historic Preservation Board and the Town of Erie Historic Preservation Advisory Board. Other groups that may potentially join the working group include organizations such as the Boulder Historical Society, Historic Boulder, Inc., the Erie Historical Society and the Lafayette Historical Society. Veterans' organizations would also be invited, including American Legion Post 10 (Boulder) and Veterans of Foreign Wars (VFW) Post 1771 (Lafayette).

The working group will strive to come to a consensus regarding a preferred preservation approach for the pillars; however, if consensus is not achievable, CDOT will make a decision about the appropriate course of action based on the various alternatives considered by the Working Group. Cost estimates will be obtained for each alternative, and potential funding sources will be identified. At the culmination of the process of identifying and analyzing long-term preservation alternatives, the group will create a plan document to guide future actions. The preservation plan will be the end product that satisfies mitigation of the adverse effect to the stone pillars. The plan will provide a blueprint for preservation of the pillars, but would not include actual implementation of any restoration, maintenance work, or relocation. In terms of potential relocation, CDOT will include a plan element requiring creation of a detailed photographic record as well as detailed elevation drawings should relocation be the preferred alternative. Since the pillars remain in the Arapahoe Road ROW and therefore are owned by CDOT, the execution of the preservation plan will require close coordination with CDOT, and especially with the

CDOT Region 4 Senior Historian. We welcome your thoughts about our proposed mitigation, which we believe will facilitate preservation of this historically and architecturally significant resource.

We welcome your review of our historical resource determinations and of the proposed mitigation. If you have any questions or require additional information, please contact CDOT Region 4 Historian Jason Marmor at (970) 219-9155 or [jason.marmor@state.co.us](mailto:jason.marmor@state.co.us).

Sincerely,

*James Eussen*

James Eussen  
Region 4 Planning and Environmental Unit Manager

*Enclosures:*

- CDOT-prepared OAHP Inventory form for 5BL.432 with attachments
- 1980 inventory form for 5BL.432
- Draft "Nine Mile Corner Monument" report by William Meyer
- Plan sheets for 2021 intersection improvement project
- Right-of-Way plan sheets for 1983 intersection improvement project

Cc: Vanessa Santisteven

# Architectural Inventory Form

- |       |   |
|-------|---|
| _____ | Determined Eligible- NR                 |
| _____ | Determined Not Eligible- NR             |
| _____ | Determined Eligible- SR                 |
| _____ | Determined Not Eligible- SR             |
| _____ | Need Data                               |
| _____ | Contributes to eligible NR District     |
| _____ | Noncontributing to eligible NR District |

## I. IDENTIFICATION

1. Resource number: **5BL.432**
2. Temporary resource number: **N/A**
3. County: **Boulder**
4. City: **None; unincorporated Boulder County**
5. Historic building name: **Road of Remembrance Gateway**
6. Current building name: **Arapahoe Road gateway pillars**
7. Building address: **State Highway 7 (SH 7) /Arapahoe Road, just west of U.S. Highway 287 (North 107<sup>th</sup> Street) intersection, Boulder County**
8. Owner name and address: **Colorado Department of Transportation  
2829 W Howard Place, Denver, CO 80204**

## II. GEOGRAPHIC INFORMATION

9. P.M. 6th Township 1N Range 69 W  
NE ¼ of NE ¼ NE ¼ of NW ¼ of section 34
10. UTM references  
North pillar: Zone 13; 4429418 mE ; 491152 mN  
South monument: Zone 13; 4429392 mE ; 491150 mN
11. USGS quad name: *Erie, Colorado*  
Year: 1967; Revised 1971 Map scale: X 7.5' 15'
12. Lot(s): **Not applicable**  
Block: **Not applicable**  
Plat: **Not applicable**  
Year Platted: **Not applicable**  
Parcel Number: **Not applicable; the north and south pillars are located in CDOT right-of-way for State Highway 7 (Arapahoe Road)**
13. Boundary Description and Justification: **The site boundary is defined as the footprints of both (north and south) pillars flanking Arapahoe Road/SH 7. Arapahoe Road itself is not included in the property boundary for the pillars, since the roadway the pillars originally flanked has been greatly altered and has no historic integrity. In 1983, the north pillar was accidentally destroyed and rebuilt. The rebuilt pillar now stands along the north side of Arapahoe Road roughly 40 feet north of its original 1928-1983 location.**

### III. ARCHITECTURAL DESCRIPTION

14. Building/structure plan (footprint, shape): Irregular; square-plan pillars/towers with attached and projecting (2 per pillar, on opposing sides) decorative and functional stone buttress walls. At the ends of these buttress walls are low, squat square-plan stone piers that add stability to the walls and pillar structure.

15. Dimensions in feet (pillar only): Length: **Approx. 10 ft.** x Width: **Approx. 10 ft**  
Height: **Approx. 30 ft.**
16. Number of stories: **Not applicable**
17. Primary external wall material(s): **Stone – multi-colored sandstone flagstone pieces set with mortar in coursed ashlar pattern**
18. Roof configuration: **Not applicable**
19. Primary external roof material: **Stone**
20. Special features: **Segmental stone arch lintels above doors.**
21. General architectural description: **Please refer to the attached captioned photographs. This property consists of two nearly identical tower-like pillars or monuments flanking east-west Arapahoe Road (State Highway 7) close to its intersection with the west side of North 107<sup>th</sup> Street (US Highway 287). They stand on the north and south sides of Arapahoe Road, and originally formed a formal gateway to a nine mile-long direct automobile route to Boulder from the Lincoln Highway (now US 287).**

The pillars were also associated with plans to promote and develop Arapahoe Road from the Lincoln Highway to Boulder into a proposed “Road of Remembrance” referring to the sacrifices of American soldiers in World War I. Both original pillars were erected at the same time (one after the other) and are identical in terms of design, dimensions and construction materials. They present a stately monumental and symmetrical appearance.

The pillars are constructed of locally quarried tabular pieces of multicolored sandstone (flagstone) of various sizes and thicknesses set in mortar and laid in rough courses of ashlar stonework. Each pillar structure consists primarily of a tall, square-plan tower (foundation type unknown), approximately 30 feet tall, and each is equipped with a small internal chamber sealed with small (approximately 4-5 feet high) hinge-hung painted wooden door (likely non-original). These doors are kept locked by CDOT and the chamber interiors were not accessed as part of this 2021 site re-visitation. Stone segmental arches are placed above these small doorways, using vertically oriented pieces of tabular sandstone. With the exception of one small chamber door installed on each pillar, the pillars lack openings, and display elaborate fine stonework. Defining characteristics include tall and very narrow recessed vertical bays that contain what appears to be fixed glass panes (possibly an original design feature) that were subsequently covered in brown paint. Closer inspection of these vertical bays would be necessary to identify the material installed within them, along with archival and historical photograph research.

At the base (northeast corner) of the southern pillar is a cut white limestone cornerstone with the inscribed date “1928” as well as the words “Boulder Lions Club” and the organization’s official seal. Across the road, the northern pillar has a large bronze plaque affixed to its east side. This plaque was affixed to the east side of the original pillar in 1928, and was reinstalled when the pillar was moved, damaged, and reconstructed (1983) in its current location. Placed at the formal dedication of the pillars in June 1928, the plaque’s text reads: “Road of Remembrance Gateway / In Honor of Those Who Served in the World War / Erected by the Lions Club of Boulder, 1928.”

Attached to the east end west sides of each pillar are approximately 12-14 ft-long, low coursed stone ashlar buttress walls, at the far ends of which are attached squat square-plan ashlar stone pillars, each topped by large custom-designed cast concrete scrollwork decorative elements. These walls appear to provide stability and strength to help support the pillars, and are curved

and expanded where they attach to the side of the pillar. Decorative cast concrete capital elements are placed atop the top of each buttress wall.

- 22. Architectural style/building type: **Collegiate Gothic-influenced/road gateway pillars**
- 23. Landscaping or special setting features: **The historic stone pillars are located within a formerly agricultural area of unincorporated Boulder County, and a large parcel on the south side of Arapahoe Road is former farmland that is the site of an impending construction of a large commercial development. An older small rural residential property lies on the north side of Arapahoe Road and is partially obscured from view by mature deciduous trees.**

As a result of intersection improvement projects in 1983 and 2021, Arapahoe Road was been greatly widened and slightly realigned at the junction with North 107<sup>th</sup> Street/US 287 in 1983 and 2021 to improve turning movements and increase vehicle capacity and safety. In order to preserve the south pillar in situ and still construct new right turn lanes, the 2021 project included the construction of a raised, irregularly-shaped "pork chop" traffic island surrounding the south pillar, sited between the main Arapahoe Road eastbound travel lane and the new right turn lane, which carries traffic behind and past the pillar. While the pillar was left in situ, it now sits isolated within the intersection, and its gateway purpose is not readily discernable in part because the northern pillar was relocated 40 feet north of its original location in 1983 and now sits obscured by trees. Currently the connection between the north and south pillars is not obvious.

Modern commercial and residential development in unincorporated Boulder County is transforming the agricultural landscape and modernizing the character of the area including the vicinity of the Arapahoe Road/UD 287 intersection. Former low density agricultural land is undergoing development, and this activity has greatly changed the historic setting of the Arapahoe Road stone pillars.

- 24. Associated buildings, features, or objects: **Arapahoe Road/SH 7**

#### **IV. ARCHITECTURAL HISTORY**

- 25. Date of Construction: Estimate:    Actual: **1928 (April-June) - groundbreaking and laying of cornerstone on April 18, 1928; dedicated June 17, 1928**  
Source(s) of information: **(Meyer 2021:10)**
- 26. Architect: **Meade Walter**  
Source(s) of information: **(Meyer 2021:10)**
- 27. Builder/Contractor: **Lee Roy Watson (stone mason)**  
Source(s) of information: **(Meyer 2021:10)**
- 28. Original owner: **Lions Club of Boulder**  
Source(s) of information: **(Meyer 2021:10)**
- 29. Construction history (include description and dates of major additions, alterations, or demolitions): **In 1928, architect and Lions Club member Meade Walter designed the pillars at "Nine Mile Corner" (Lincoln Highway/US 287 and Arapahoe Road intersection) as a war memorial honoring the 1,000 Boulder County residents who served in World War I, including the architect's son. The pillars closely flanked Arapahoe Road, which was much narrower than at present. The planned memorial pillars would be placed on opposite (north and south sides) of Arapahoe Road to serve as a gateway to the road to Boulder, which was proposed as a World**

War I memorial "Road of Remembrance." According to the architect, the pillars would be built of flagstone, and the design and stonework were intended to replicate "the walls and alcoves of the new University buildings."

Work on constructing the pillars took two months to complete. On April 18, 1928 ground was broken and a cornerstone was laid at the base (northeast corner) of the south pillar. Stone mason Lee Roy Watson built the multi-colored sandstone structures. Watson also is responsible for constructing some of the stone structures at St. Malo in western Boulder County possibly including the impressive stone St. Malo Church/Chapel near Allenspark, Colorado.

The completed stone pillars were dedicated on June 17, 1928. Fifty-five years later, in 1983, a Colorado Highway Department (CHD) intersection improvement project was undertaken, and involved substantial widening on the north side of Arapahoe Road. Flatiron Paving Company (Flatiron) was hired by the Highway Department to perform the intersection improvement work on Arapahoe Road. To accommodate the new intersection design, in April 1983 Flatiron subcontracted with Whitlock House Moving of Louisville to move the north pillar 40 feet to the north of its original location. While digging into the sides of the north pillar in order to jack it up and place steel beams beneath it – which were to be used to slide the pillar to its new location – the sandy soil gave way and the pillar tipped over and slowly fell to the ground and broke to pieces. Flatiron agreed to faithfully rebuild the pillar. The reconstruction plan involved cleaning the unbroken flagstone, and replacing the broken stones with flagstone of matching color. Since Meade Walter's original design plans for the pillars were not available, the damaged pillar was re-assembled and repaired to match the intact south pillar using similar construction methods and materials. Flatiron Paving hired Del's Masonry of Longmont to rebuild the pillar. The work was completed by a couple from Del's Masonry, Lorri and Roy Martinez, in the summer of 1983, and approximately 80% of the original stones were utilized. Costing around \$20,000, the effort took several weeks to finish. The south pillar remains in its original location, but in 2021 a raised "pork chop" traffic island was constructed around it.

30. Original location X (south pillar) Moved X (north pillar)  
Date of move(s): 1983

## V. HISTORICAL ASSOCIATIONS

31. Original use(s): Recreation and Culture - War memorials; Transportation – highway gateway  
32. Intermediate use(s): None  
33. Current use(s): No current use  
34. Site type(s): War memorials; road/highway gateway monuments  
35. Historical background: (Note: Please refer to the unpublished report by William Meyer (2021) for a detailed history of Nine Mile Corner including the war memorial gateway pillars. The following summary includes excerpts from this report.)

The stone pillars currently flanking Arapahoe Road just west of its intersection with U.S. 287 were originally constructed in 1928. Their construction was driven by two different motives. They were envisioned and built as part of a formal, imposing gateway to serve as war memorials flanking a proposed memorial "Road of Remembrance" (Arapahoe Road). Additionally, the gateway, of which the tall stone pillars were the primary feature, was envisioned by civic and business boosters as an effective way to divert traffic traveling on the Lincoln Highway (U.S.

**287) between Longmont and Lafayette by enticing motorists to turn west onto Arapahoe Road and visit the City of Boulder.**

In the 1910s and 1920s, when auto tourism was in its infancy, the Lincoln Highway was one of the most publicized transcontinental roads for American motorists. Businessmen and civic leaders in Boulder were interested in development of the Lincoln Highway to boost tourism and commerce. In the late 1910s and early 1920s Boulder lay ten miles west of the Lincoln Highway, with poor access to the growing commercial traffic along the Lincoln Highway. Boulder's connection to the Lincoln Highway was Arapahoe Road, which intersected the Highway at "Ten Mile Corner." Also known as the Valley Road, in the late 1910s and early 1920s, Arapahoe Road was an unpaved county road traversing the plains into the city.

As automobile use increased dramatically after World War I, the need for more and better automobile roads nationwide spurred a "Good Roads" movement, which involved local action to develop new safe automobile routes connecting communities. In 1919 the Boulder Rotary Club created a "Good Roads Committee" to pursue improving the roads in Boulder County. Working with local commercial associations in Boulder and Longmont, the committee proposed an ambitious road improvement program that included paving the entire length of the Lincoln Highway in the county, and Arapahoe Road from Ten Mile Corner to the city.

Another nationwide movement was gaining momentum concurrently: developing roads into named/designated World War I memorial travel routes, or "roads of remembrance" featuring beautification including tree plantings along the margins of the roadway as promoted by the American Forestry Association. The idea of honoring Americans who participated in World War I with roads of remembrance was championed in newspaper editorials around the country, and became a popular idea, resulting in various plans for roads of remembrance in the United States.

Boulder County embraced the road of remembrance movement, and in January 1923 the Boulder Commercial Association, a variety of the Boulder service clubs and the Boulder County Commissioners began working on the development of a "Road of Remembrance" on Arapahoe Road from Boulder to Ten Mile Corner, dedicated to the soldiers from the county who fought in WWI. The preliminary plan included construction of a gateway arch or monument at Ten Mile Corner, along with planting trees and possibly lights on both sides of the road. Boulder American Legion Post 10 quickly assumed sponsorship of Boulder County's proposed "Road of Remembrance." By 1923, Boulder's business community and civic organizations supported the plan for a "Road of Remembrance" to connect with the Lincoln Highway.

The Lincoln Highway became the main north-south road in Boulder County and from northern Colorado to Denver and carried growing tourist traffic in the 1920s. With the increase in automobile travel and the higher speeds of newer cars, the Highway's zig-zag route from Ten Mile Corner to the beginning of the pavement south of Longmont was quite dangerous. Faced with this hazardous situation, in late 1923 the Boulder County Commissioners passed a resolution intended to eliminate these dangers by relocating a portion of the Lincoln Highway (US 287) by constructing a new, straight six-mile stretch of north-south highway south of Longmont, approximately 0.75 mile west of 111th Street. The proposed new highway alignment did not follow existing roads, and required obtaining land for a right of way from farmers along the route.

Construction of the new highway began in 1926 and was completed in 1927, after which focus shifted to the proposed gateway at Nine Mile Corner. As the plan for the gateway evolved, the initial idea of a big entryway arch was abandoned, and the gateway design was changed to include a pair of stone pillars flanking Arapahoe Road, at the east end of the proposed "Road of Remembrance," as well as a small park on the triangular tract formed by the roadways. The Boulder Lions Club, which funded construction of the pillars, wanted them to "mark the new route" opened by the relocated Lincoln Highway, as well as serve as a suitable memorial in honor of those who served our country in World War I. The Road of Remembrance pillars were loosely patterned after the gateway pillars at the entrance to Lookout Mountain Park near Golden.

Plans for the pillars were drawn by Meade Walter, an architect and Lions Club member, whose son had served in the First World War. The pillars would be built of flagstone, and according to Walter it was "designed something like the walls and alcoves of the new University buildings [in Boulder]."

Ground was broken and the cornerstone for the pillars (south pillar) was laid on April 18, 1928. Construction of the pillars took two months. The work was completed by stone mason Lee Roy Watson, who is credited with constructing some of the stone structures at St. Malo in western Boulder County. The pillars were officially and ceremoniously dedicated on June 17, 1928, at which time a small box of documents and mementoes were placed in a niche in the cornerstone.

While work was proceeding on the pillars, preparations were underway for construction of the second phase of the gateway project - the triangular memorial park immediately east of the pillars. A year after the pillars were completed, Boulder American Legion Post 10 constructed the park in the middle of the "wye" at the intersection. The Legion's plan for the park centered on the captured (and deactivated) artillery pieces donated by United States government. On May 19, 1929, American Legionnaires installed a solitary 100 mm captured German cannon on a concrete base in the leased triangular tract of land, and also erected a donated 48' flagpole. Trees and shrubs were planted in the triangular plot, which was surrounded by barriers comprised of low posts and cables. The shoulder of the roadways of the arcs comprising the "wye" were sufficiently wide to permit parking for those accessing the park and the pillars.

After completion of the gateway park, support for "Road of Remembrance" project dissipated, and the planned planting of 1,000 trees along the route never came to fruition. Boulder County's interest in the "Road of Remembrance" diminished, and the concept was never fully realized. Although the war memorial pillars and adjacent small park were installed at the road's entrance, none of the other proposed amenities along the route (such as trees) were ever installed, and Arapahoe Road was never formally named or called the "Road of Remembrance."

The gateway pillars remained standing in their original locations until 1983, when the CHD improved and realigned Arapahoe Road. The intersection project expanded the highway right-of-way to the north, and required the north pillar to be relocated 40 feet to the north of its original location. While moving the north pillar it tipped over and when striking the ground broke apart. The project contractor hired a specialized company to faithfully reconstruct the north pillar.

C. 2019-2020, a development company seeking to construct a large new commercial development in the southwest quadrant of the Arapahoe Road/US 287 intersection planned



intersection improvements, including new right-in and right-out turn lanes. Knowing that CDOT would not allow the relocation of the south pillar based upon its historical significance, the developers' roadway design consultant, Galloway, chose to construct a raised "pork chop" traffic island with guard rail surrounding it. The intersection work was completed in 2021. Consequently, the south pillar now sits within Arapahoe Road rather than on the south side of and flanking the road. Since the 2021 intersection improvement project is situated within the State-owned right-of way for Arapahoe Road, CDOT provided the developers with an access permit to complete the work.

After the project was brought to the attention of CDOT's Region 4 Senior Historian (Jason Marmor) by Mr. William Meyer, on behalf of a Boulder Rotary Club working group that is seeking to ensure preservation and periodic inspection and maintenance of the pillars. In accordance with the Colorado State Register Act, CDOT conducted "after-the fact" consultation with the State Historic Preservation Officer (SHPO) and stakeholder organizations (e.g. veterans' groups and historic preservation boards) regarding the significance of the stone pillars and the effect of the 2021 intersection improvement project upon them. CDOT determined that, despite the loss of some integrity, the project caused an adverse effect that would require mitigation. CDOT proposed mitigation consisting of a collaborative process to develop a preservation plan document for the pillars, including consideration of relocation alternatives. The collaborative process would involve CDOT, the SHPO, and various interested governmental agencies, historic preservation boards, veterans' groups, and service organizations. Execution of recommendations from the preservation plan document will depend on funding, leadership and effective collaboration among the interested parties.

36. Sources of information:

Blakeslee, David Allen

1983 Warranty Deed from David Allen Blakeslee to the Colorado Department of Highways, for ownership transfer of a 0.142-acre parcel in the SE ¼ of the SW ¼ of Section 27, Township 1 North, Range 69 West, in Boulder County, Colorado. The parcel was designated Parcel No. 5 in plans for Colorado Highway Department Project No. FC-HES 007-2(5). Recorded at the Boulder County Clerk and Recorder's Office on March 7, 1983. Reception No. 536538.

Daily Times

1926 "Plans Nearing Completion for Right-of-Way Highway; Local Engineer Working on New Route." The Daily Times, Longmont, Vol. XXXII, Number 60, February 25, 1926.

Garten, Carol

1983 "Monument on Highway 7 – 287 Comes Tumbling Down." The Louisville Times, Vol. 69, No. 44, April 20, 1983.

Meyer, William

2021 Nine-Mile Corner Monument (narrative historical report), 26 pp., unpublished.

**Town of Erie**

**1983 Warranty Deed from the Town of Erie, Colorado to the Colorado Department of Highways, for ownership transfer of a 0.324-acre parcel in the NE ¼ of the NW ¼ of Section 34, Township 1 North, Range 69 West, in Boulder County, Colorado. The parcel was designated Parcel No. 6A in plans for Colorado Highway Department Project No. FC-HES 007-2(5). Recorded at the Boulder County Clerk and Recorder's Office on February 8, 1983. Reception No. 532304.**

**Weiss, Manuel**

**1980 Colorado Cultural Resource Survey Inventory Record for Site 5BL.432, "Road to Remembrance Gateway." Boulder County Historical Society, May 12, 1980.**

**VI. SIGNIFICANCE**

37. Local landmark designation: Yes \_\_\_\_ No **X** Date of designation: **Not Applicable**  
Designating authority: **Not Applicable**

38. Applicable National (and State) Register Criteria:

- X** A. Associated with events that have made a significant contribution to the broad pattern of our history;  
\_\_\_\_ B. Associated with the lives of persons significant in our past;  
**X** C. Embodies the distinctive characteristics of a type, period, or method of construction, or represents the work of a master, or that possess high artistic values, or represents a significant and distinguishable entity whose components may lack individual distinction; or  
\_\_\_\_ D. Has yielded, or may be likely to yield, information important in history or prehistory.

\_\_\_\_ Qualifies under Criteria Considerations A through G (see Manual)

\_\_\_\_ Does not meet any of the above National Register criteria

39. Area(s) of significance: **Social History, Architecture**

40. Period of significance: **1928**

41. Level of significance: National \_\_\_\_ State \_\_\_\_ Local **X**

42. Statement of significance: **The pillars were originally inventoried as the "Road to Remembrance Gateway" in May 1980 by Manuel M. Weiss of the Boulder County Historical Society, as part of the Boulder County Historical Site Survey, Boulder and Broomfield Counties (Report No. MC.LG.R20). Weiss prepared a Colorado Historical Society Inventory Record, and evaluated the gateway pillars as eligible for the National Register of Historic Places (NRHP). Weiss' 1980 inventory form does not provide a narrative explanation of the site's significance in terms of the NRHP eligibility criteria. The resource has a "field eligible" determination date of May 1, 1980.**

**In the summer of 2021 CDOT revisited, rerecorded and reevaluated the pillars. They are the only extant remnants of the 1928 war memorial gateway that formerly included a small triangular park with a captured German artillery piece and flagpole. The site is historically significant under Criterion A for its association with a widespread movement in the 1920s led by fraternal organizations and philanthropic clubs to install public memorials to honor American soldiers who served during the First World War. The pillars and gateway are also associated with enthusiastic civic promotion seeking to attract and divert early motorists from the Lincoln**

Highway/US 287 to Boulder and boost the city's economy. The tall stone pillars standing on Arapahoe Road adjacent to the Lincoln Highway were visually prominent landmarks in this sparsely populated agricultural area, and they were immediately recognized by motorists as forming a gateway to what must have appeared to be an important road.

Under Criterion C, the pillars are significant for their intricate, University of Colorado at Boulder collegiate architecture-inspired design and elaborate stonework construction. They were designed by architect Meade Walter, who does not appear to have been a prominent Colorado architect. The Arapahoe Road pillars are also architecturally significant as particularly elaborate example of stone roadway gateways. There are other somewhat similar stone pillar gateways in Colorado, such as at Lookout Mountain Park near Golden.

43. Assessment of historic physical integrity related to significance: The physical integrity of the Arapahoe Road pillars has been substantially diminished as a result of two intersection improvement projects, in 1983 and 2021. During the 1983 project the north pillar had to be relocated 40 feet to the north, but during the relocation effort it tipped over and broke apart. The pillar was immediately and expertly reconstructed in the selected location 40 feet north of its original location. Although the north pillar looks exactly like the south pillar, the separation of the pillars by 40 additional feet was a major change by creating a much wider roadway than the original road and greatly diminished their appearance as gateway markers.

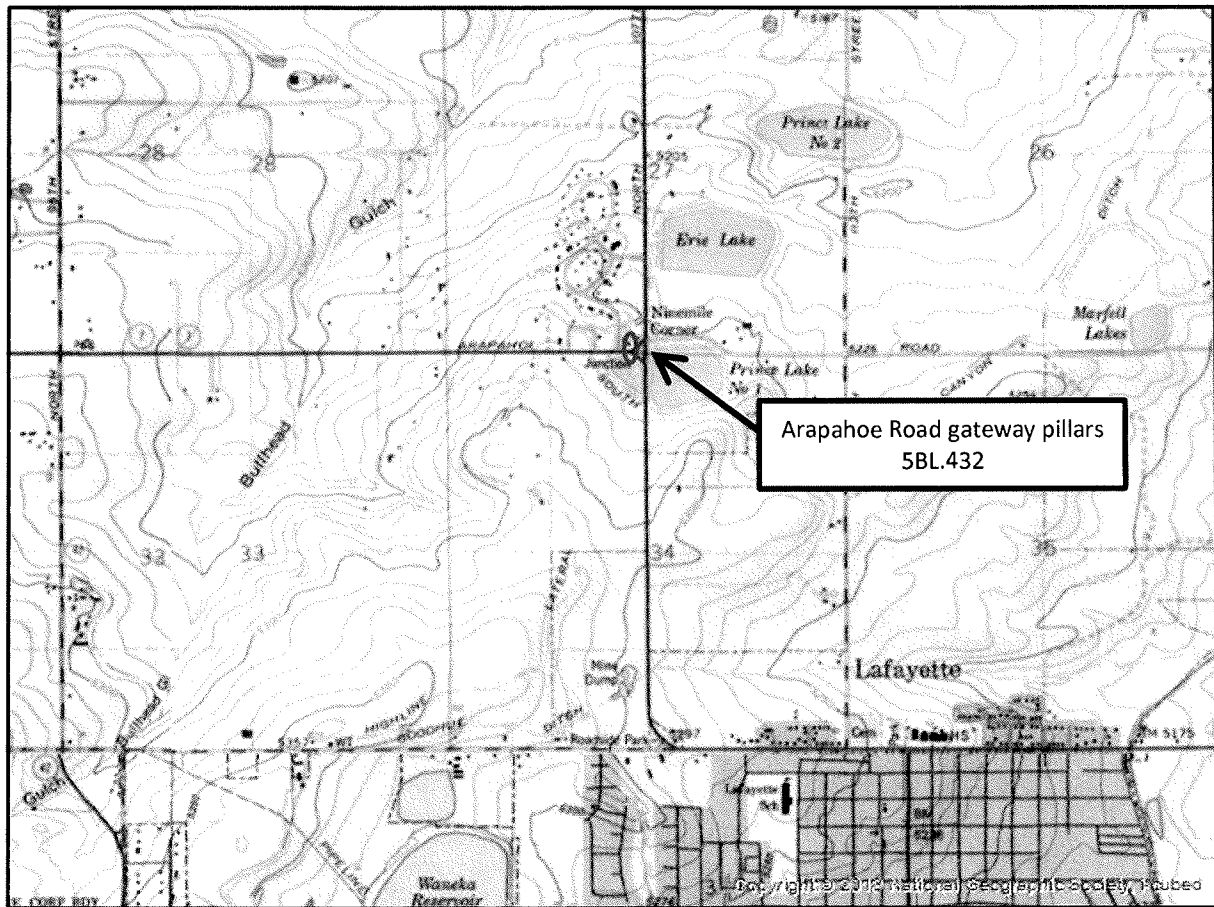
The south pillar remains intact; however recent (2021) construction greatly impacted the integrity of the historic setting - a new right turn lane was built that passes behind the pillar. Additionally, to protect the south pillar from cars, it was surrounded a raised "pork chop" traffic island equipped with guardrail; it now appears to sit within the road and adds a new and serious vehicular hazard.

## VII. NATIONAL REGISTER ELIGIBILITY ASSESSMENT

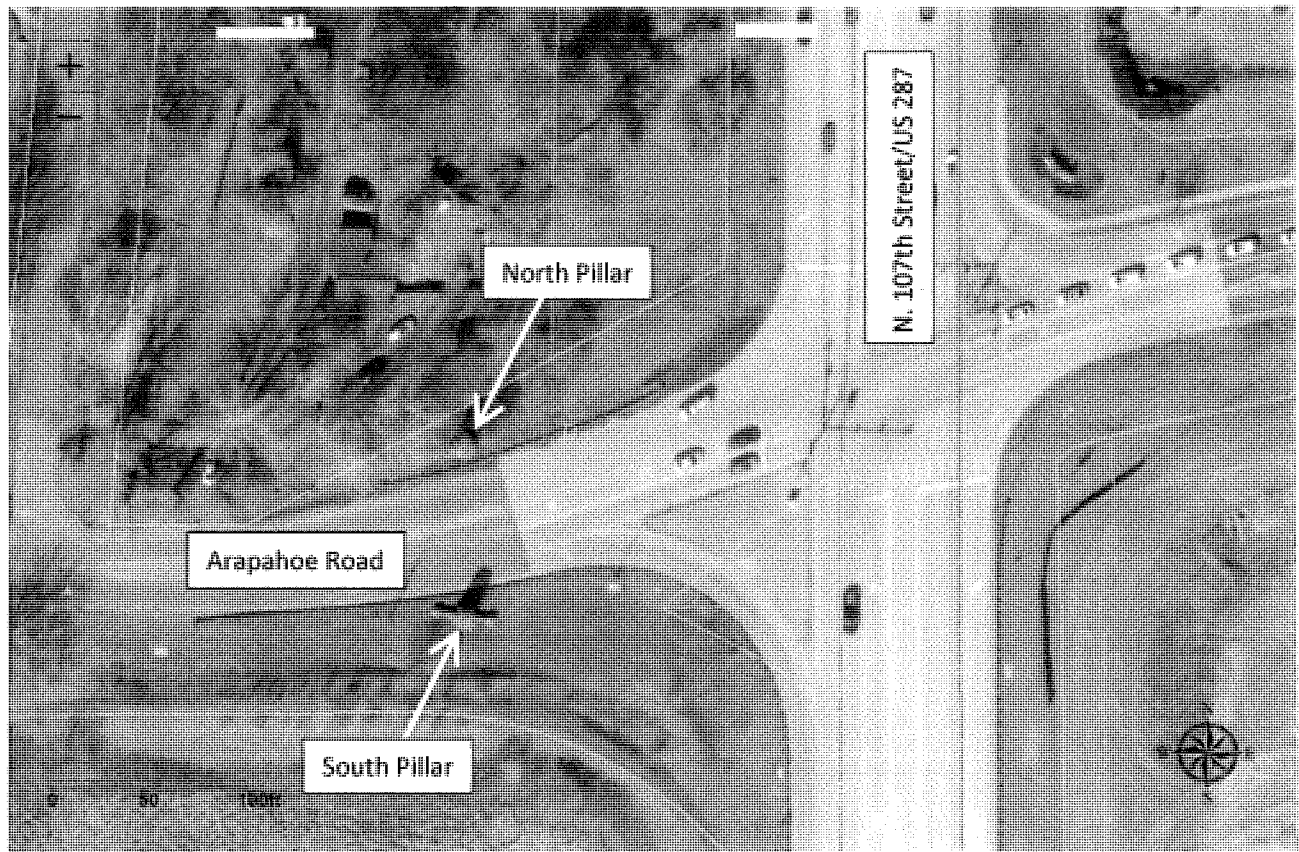
44. National Register (individual) eligibility field assessment:  
Eligible ☒ Not (Individually) Eligible ☐ Need Data ☐
45. Is there National Register district potential? Yes ☐ No ☒ Discuss: The only associated historical resource is Arapahoe Road/State Highway 7 from Nine-Mile Corner (US 287 intersection) to Boulder, which is flanked at its eastern terminus/entrance by the memorial gateway pillars. In the late 1920s this automobile road with flanking gateway pillars was conceived of as a war memorial honoring U.S. sacrifices in World War I, and was referred to as the "Road of Remembrance." Over time the commemorative name of the road was forgotten. Near the intersection with US 287, Arapahoe Road/SH 7 was widened in 1983 and even more substantially altered in 2021 to further widen and improve the intersection for safe vehicular turning movements. The 1983 intersection improvement project required the relocation of the northern pillar. The appearance and character of the highway at the location is considerably different than when the pillars were erected in 1928. Consequently Arapahoe road was not included as a contributing associated resource.  
If there is National Register district potential, is this building:  
Contributing ☐ Noncontributing ☐
46. If the building is in existing National Register district, is it:  
Contributing ☐ Noncontributing ☐ Not Applicable ☒

**IX. RECORDING INFORMATION**

- 47. Photograph numbers: **5BL.432-1 through 5BL.432-24**  
Negatives or digital photo files filed at: **Colorado Dept. of Transportation, Region 4, Greeley**
- 48. Report title: **Post-Project State Register Act Eligibility and Effect Determinations for Improvements to the U.S. Highway 287 and State Highway 7/Arapahoe Road Intersection in Boulder County.**
- 49. Date(s): **October 7, 2021**
- 50. Recorder(s): **Jason Marmor, CDOT Region 4 Senior Historian**
- 51. Organization: **Colorado Department of Transportation**
- 52. Address: **10601 West 10<sup>th</sup> Street, Greeley, CO 80634**
- 53. Phone number(s): **(970) 219-9155**



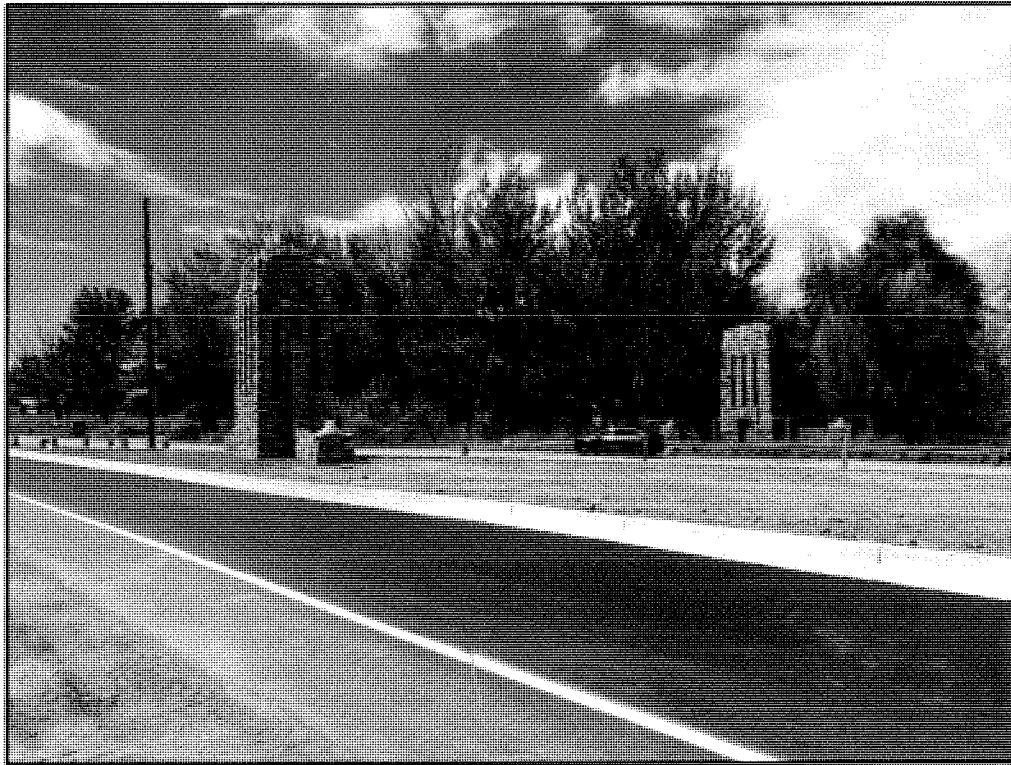
Location of the Arapahoe Road gateway pillars (5BL.432), shown on a portion of the U.S. Geological Survey 7.5' *Erie, Colorado* topographic quadrangle map (1967; revised 1971).



Sketch map of the Arapahoe Road gateway pillars (5BL.432), prior to the 2021 intersection improvement project.



Arapahoe Road gateway pillars, showing their locations and setting prior to the 2021 intersection project. View looking northwest with south pillar in foreground.



Arapahoe Road gateway pillars (5BL.432), looking west- northwest.

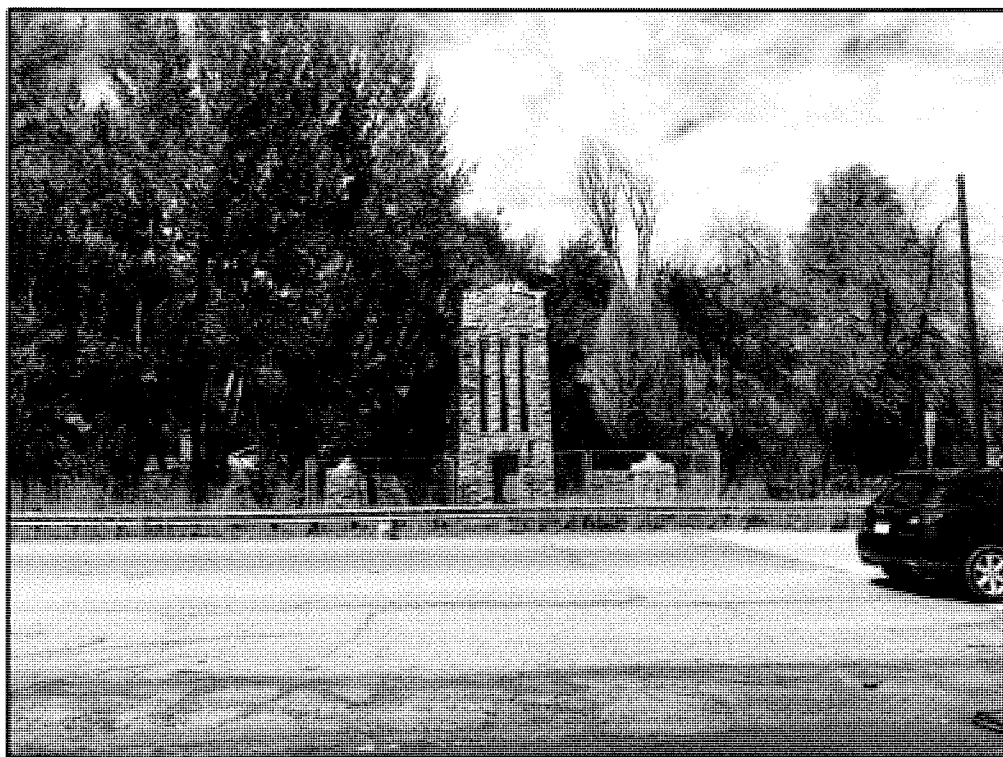


Arapahoe Road gateway pillars (5BL.432), looking northwest.





5BL.432, northern gateway pillar on Arapahoe Road/SH 7, looking northeast.



5BL.432, northern gateway pillar, looking north.



5BL.432, northern gateway pillar, looking west.



5BL.432, northern gateway pillar, looking west-northwest.



5BL.432, south side of northern gateway pillar, looking north and showing the ashlar stonework and tall, narrow window panels that are now sealed with wood.

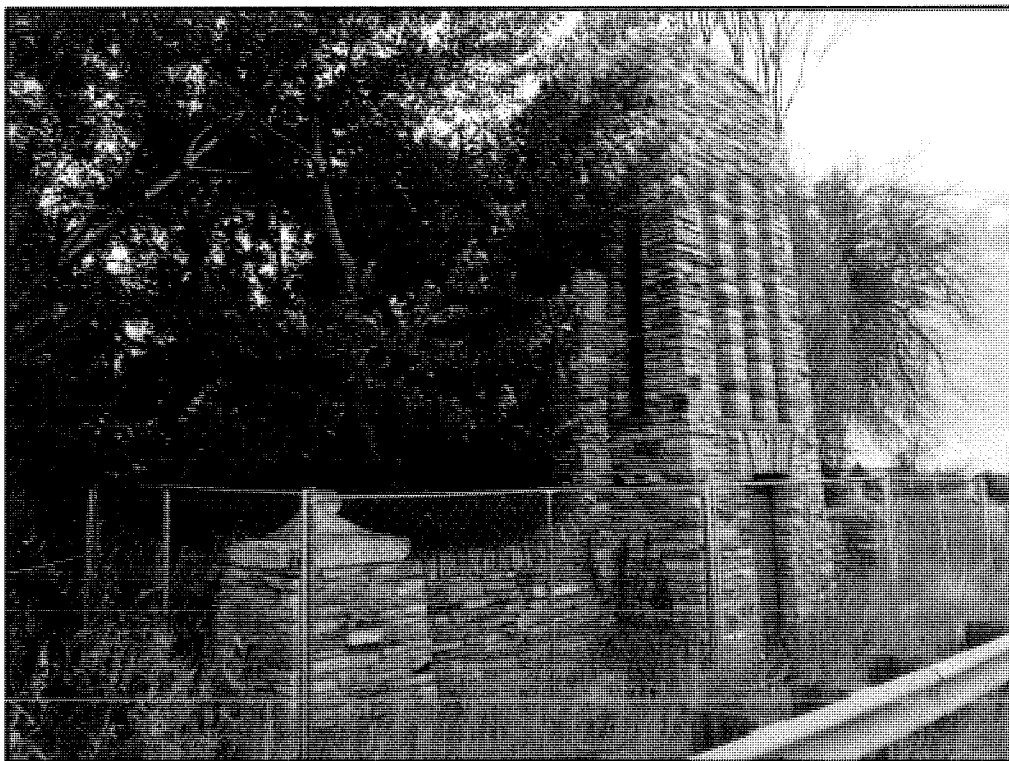




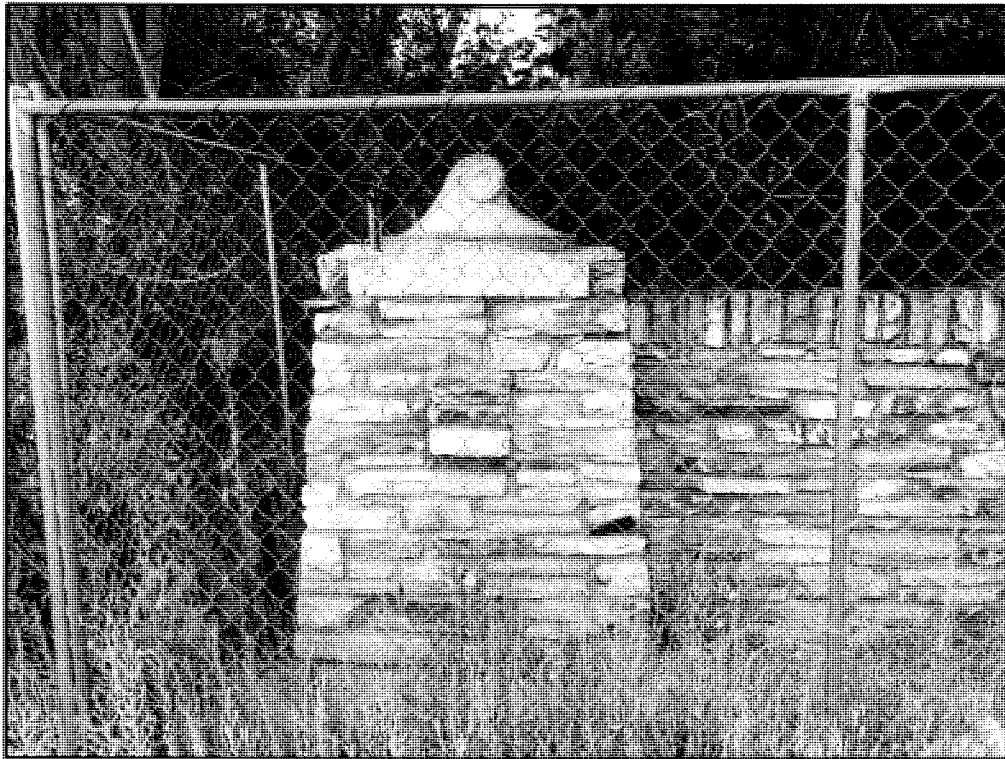
5BL.432, door on southern face of northern gateway pillar, looking northeast.



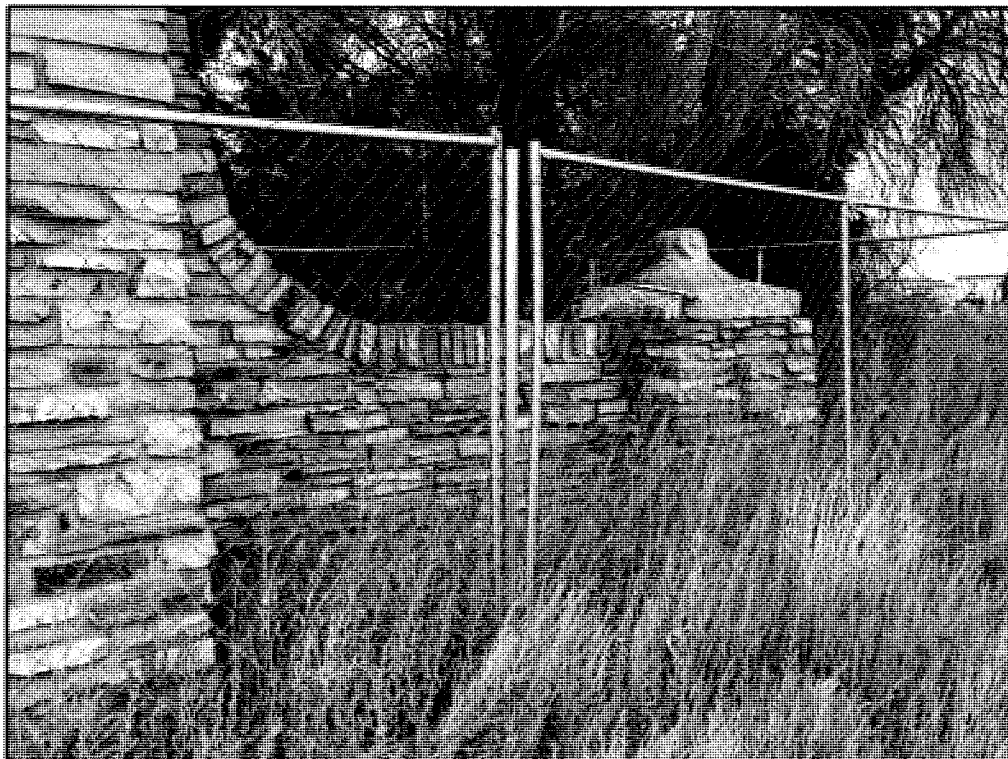
5BL.432, northern gateway pillar, looking west-southwest.



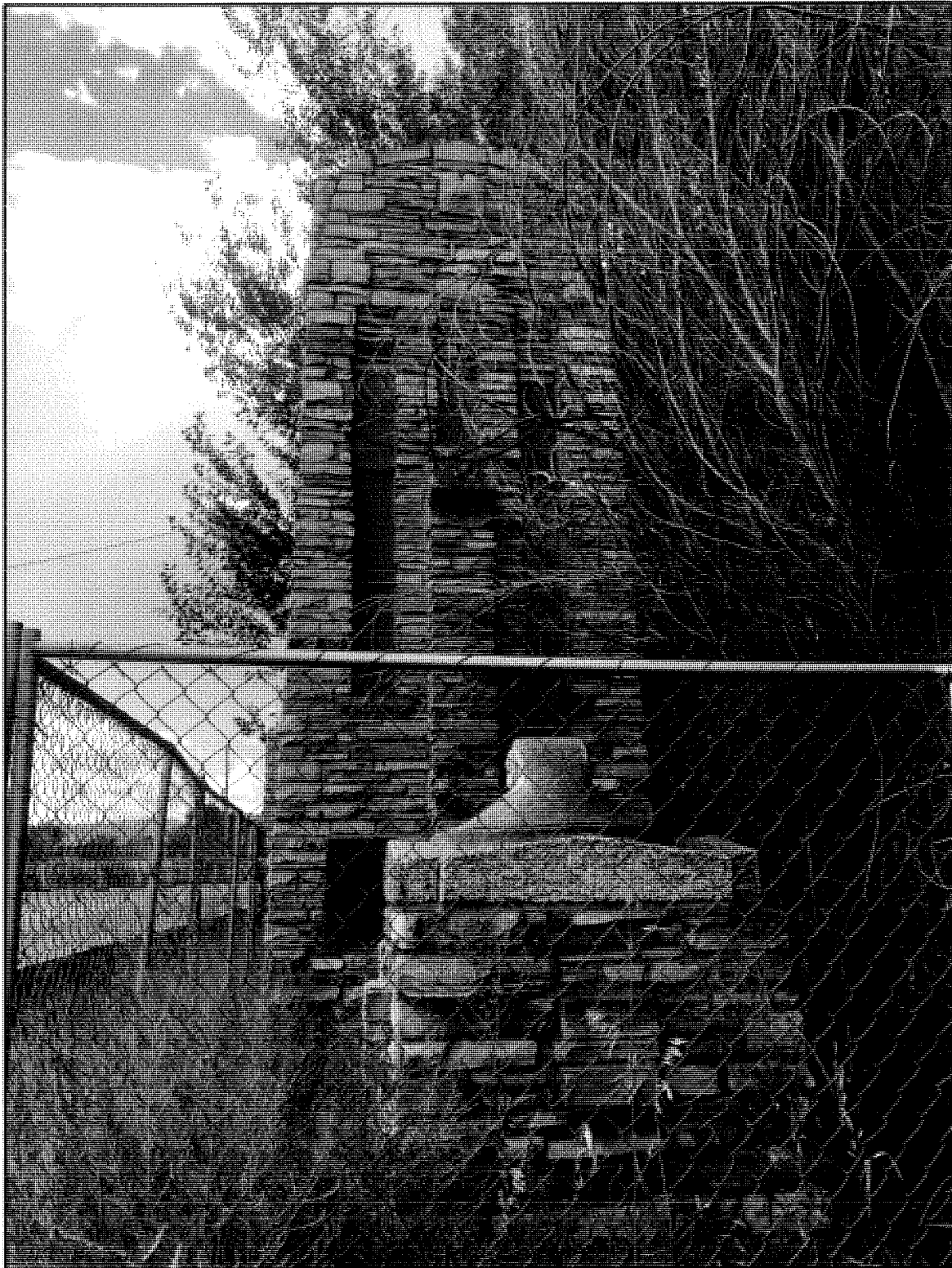
5BL.432, northern gateway pillar, looking east-northeast.



5BL.432, wall end pier topped by concrete decorative element, north pillar, looking north.

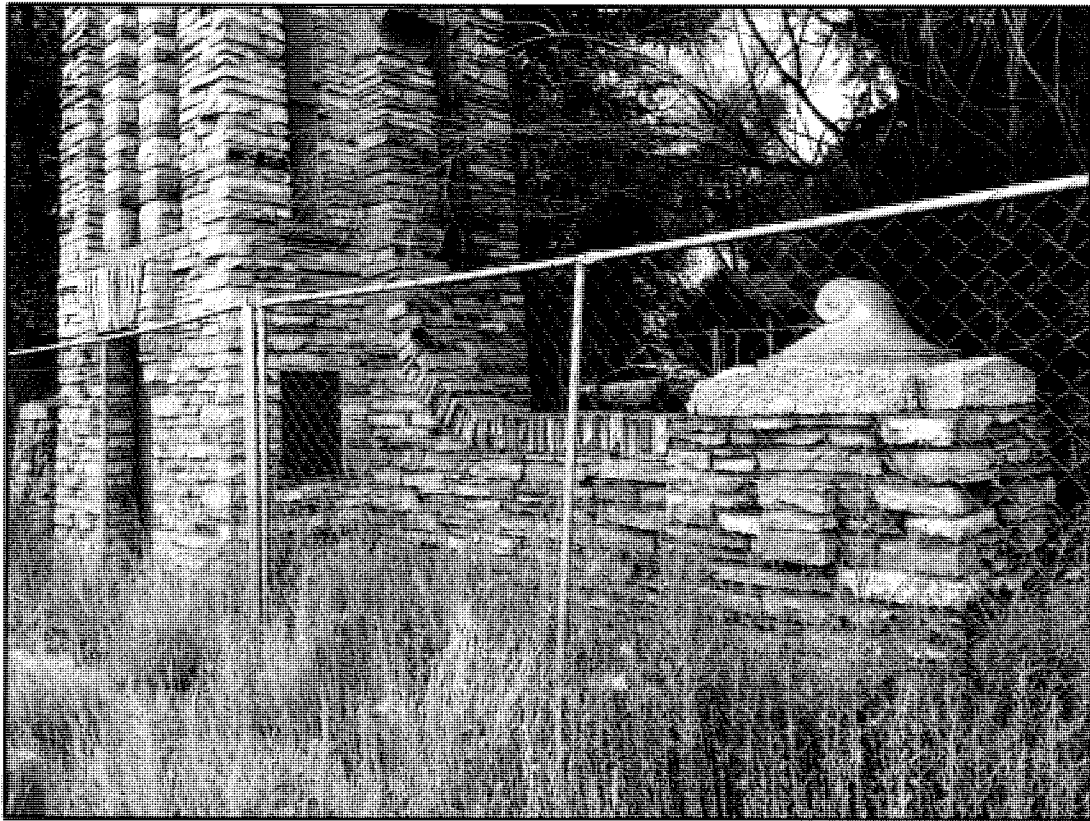


5BL.432, wall and end pier on east side of north pillar, looking northwest.

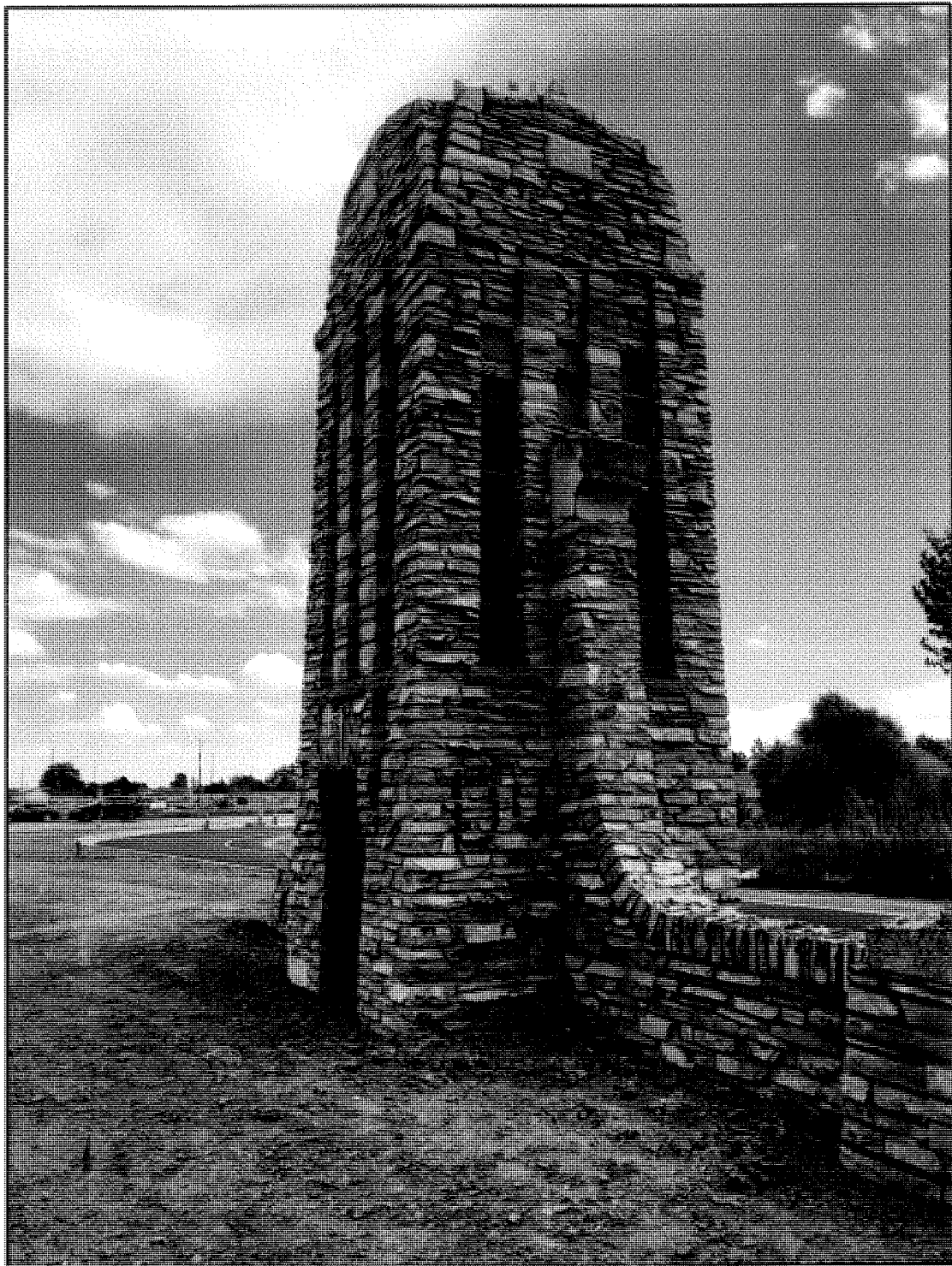


5BL.432, northern pillar, looking west, with wall end pier topped by concrete decorative element.





5BL.432, northern gateway pillar, looking northwest.



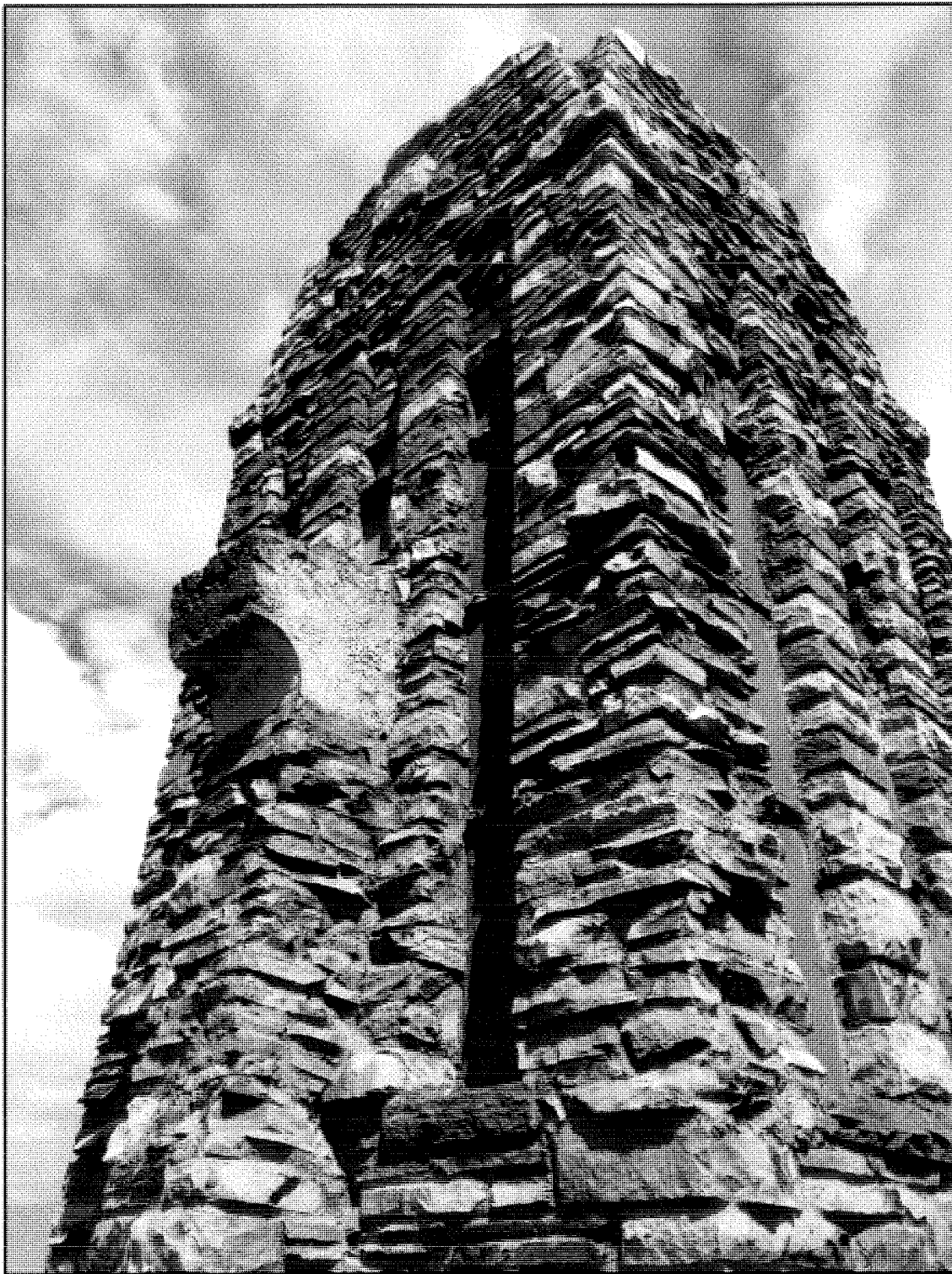
5BL.432, southern gateway pillar, looking east-southeast.



5BL.432, southern gateway pillar, looking east-northeast.

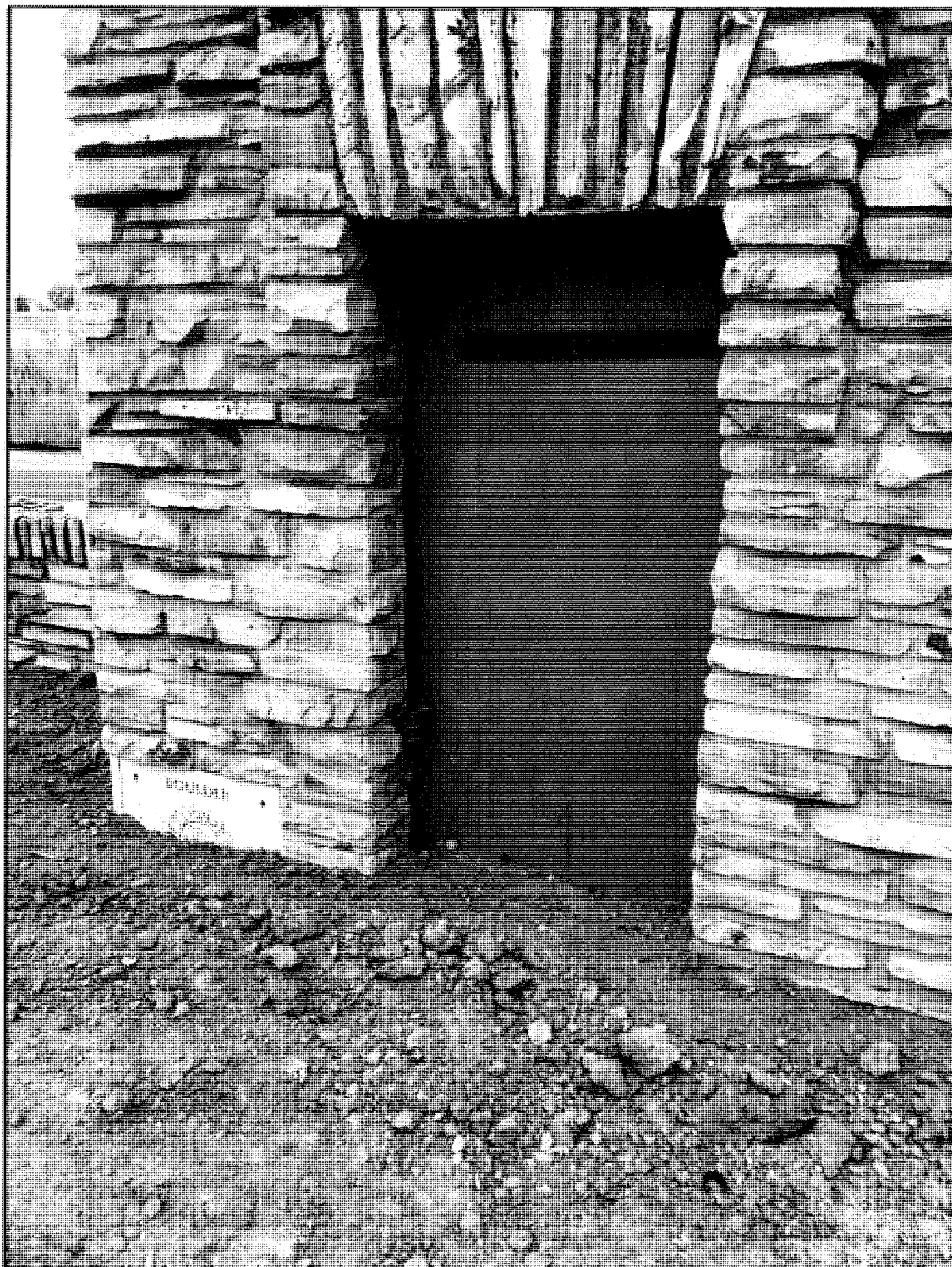


5BL.432, southern pillar, looking east.



5BL.432, close-up of ashlar stonework and concrete element on southern pillar.





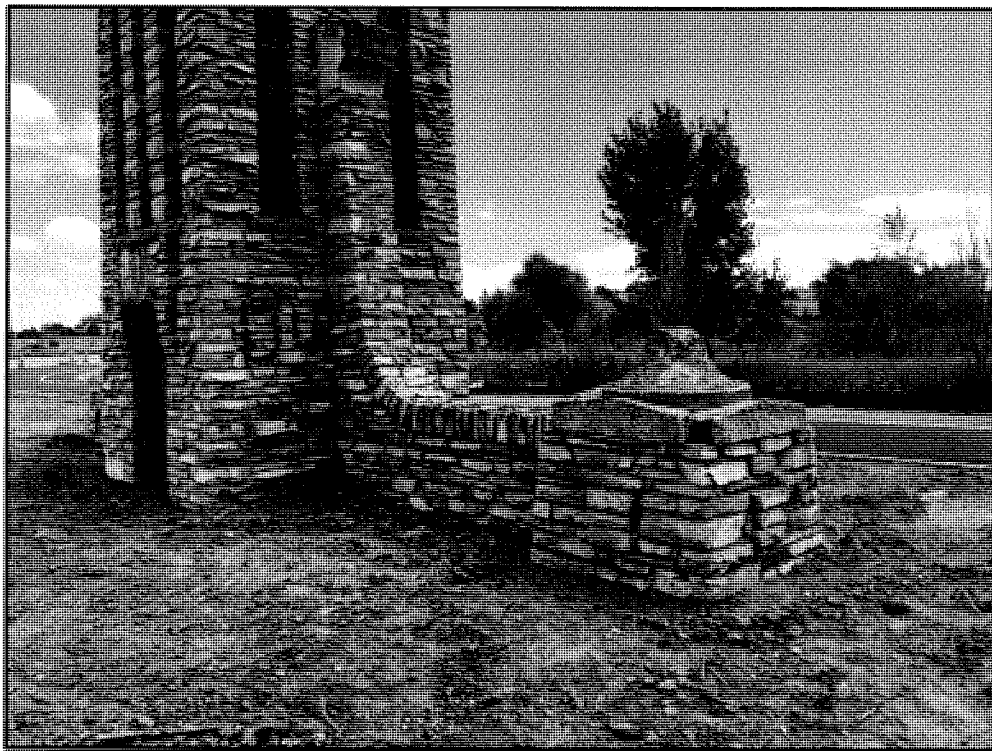
5BL.432, wooden door on north side of southern pillar, looking southeast.



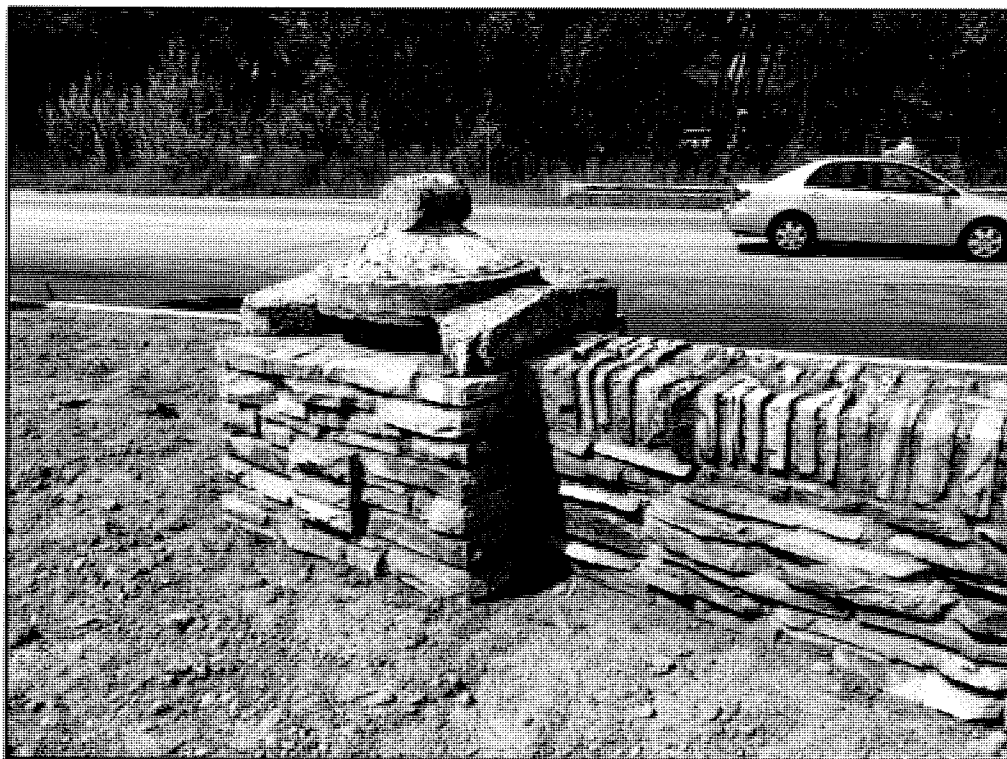
5BL.432, northern gateway pillar, looking north.



5BL.432, close-up of cornerstone at base of southern pillar.



5BL.432, west side of southern pillar, looking southeast.



5BL.432, support wall with decorative end pier of southern pillar, looking northwest.



5BL.432, southern pillar, west side pier with decorative cast concrete element, looking NNE.





5BL.432, decorative cast concrete element placed on top of end pier of southern gateway pillar, looking south.

## NINE MILE CORNER MONUMENT

Two pillars stand on Arapahoe Road at its intersection with US 287 in Boulder County. Few know that the pillars are a memorial to those who served in WWI. Fewer still know that they were built as an entrance to Boulder, or why the “entrance” was built miles from the city limits.

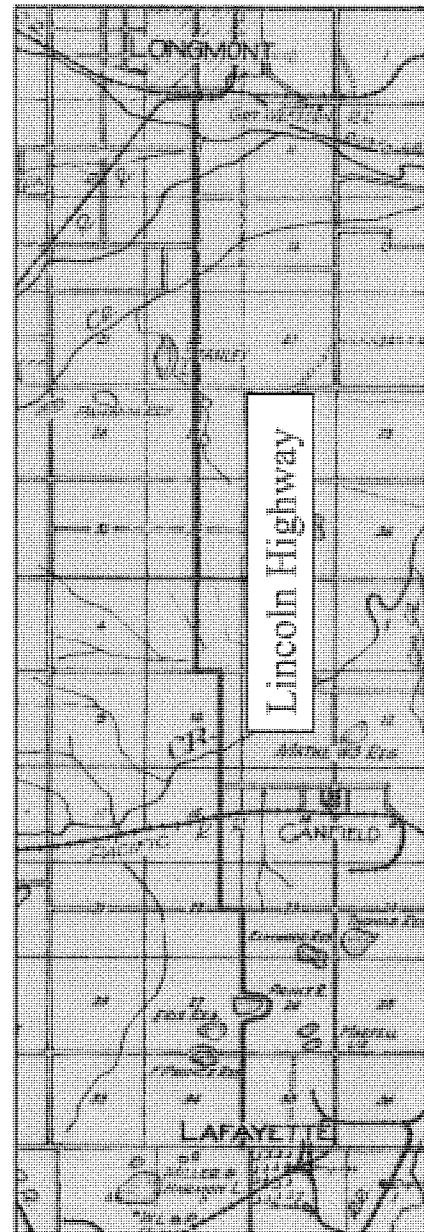
### The Lincoln Highway

The early part of the 20<sup>th</sup> century saw the rapid expansion of automobile ownership throughout the United States. With this growth came increasing demand for better roads.

In 1913, the Lincoln Highway Association was formed to promote creation of a hard-surfaced road from New York to San Francisco. While the primary route laid out by the Association’s Proclamation ran west from Omaha to Cheyenne, after furious lobbying from Colorado,<sup>1</sup> it also designated a Colorado loop heading southwest from Big Springs, Nebraska, to Denver, and then north through Longmont, Loveland, and Fort Collins, and back to Cheyenne.<sup>2</sup>

However, no “highway” existed over much of the route. To fill the void in Boulder County, local enthusiasts designated a series of unpaved county roads between Lafayette and Longmont to be the “Lincoln Highway.” This route started on the north edge of Lafayette, and proceeded north on 111<sup>th</sup> Street from its intersection with Baseline Road. After detouring around a reservoir, the route turned west on Isabelle Road, and then jogged west and north on country roads to Lookout Road, where it turned west to join 107<sup>th</sup> Street and then north to Longmont.<sup>3</sup>

In 1915, over bitter opposition from Colorado interests, the Association removed the Colorado Loop from the designated route.<sup>4</sup> Thereafter, its road guides simply noted a possible “detour” from Cheyenne to Denver along the western leg of the former Loop.<sup>5</sup> Boosters in Colorado ignored this change,<sup>6</sup> and for decades locals continued to refer to the route north from Denver to Cheyenne as the Lincoln Highway.<sup>7</sup>



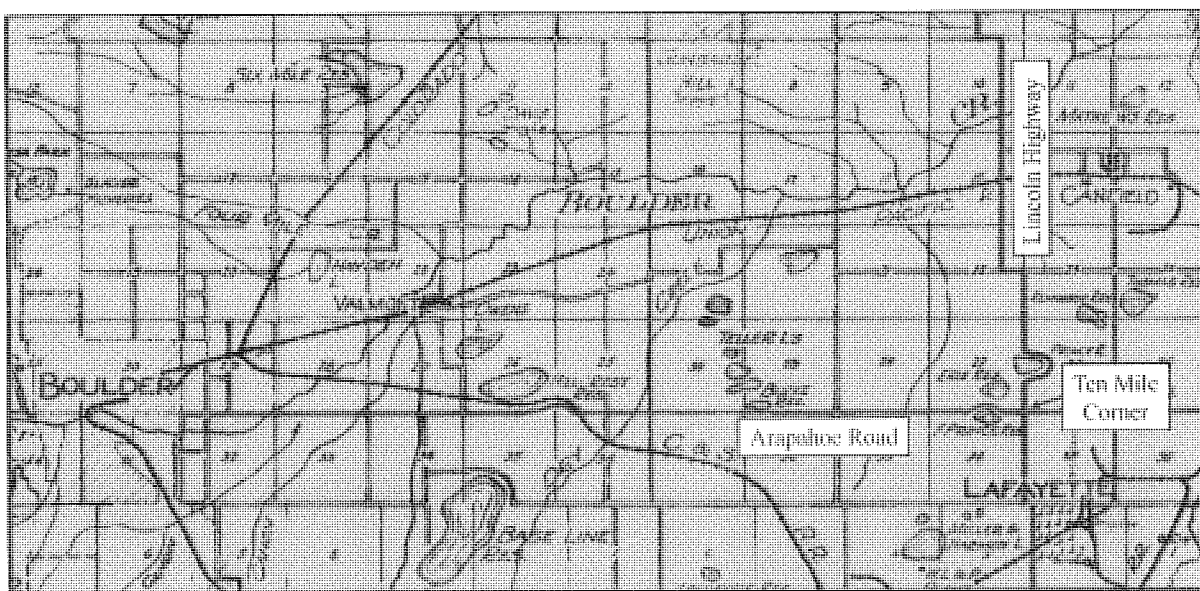
*The original Lincoln Highway<sup>8</sup>*

The creation of the Lincoln Highway coincided with another significant event in Colorado. In 1915, Rocky Mountain National Park was established outside Estes Park. Tourism had long been a significant industry in the West, with thousands of tourists arriving from the Midwest and East by train. Road trips soon began to overtake rail travel as visitors began to enjoy the mobility and freedom afforded by the automobile. By the 1920s, summer travel to Denver and north on the Lincoln Highway to Estes Park was flourishing – and lucrative for merchants along the route.

### **The Good Roads Movement and The Road of Remembrance**

One group watching the development of the Lincoln Highway were the businessmen and civic leaders in Boulder. Though it was the county seat and by far the largest town in the county, Boulder lay ten miles west of the Lincoln Highway, with poor access to the growing commercial traffic along that tourist pathway.

Boulder's connection to the Highway was Arapahoe Road, which intersected with the Lincoln Highway at the so-called Ten Mile Corner. Also known as the Valley Road, Arapahoe was an unpaved county road running across the plains into the city.



*Ten Mile Corner*<sup>9</sup>

In 1919, the Boulder Rotary Club created a “Good Roads Committee” to work on the issue of improving the roads in the area.<sup>10</sup> Working with the local Commercial Associations in Boulder and Longmont, they proposed an ambitious program to improve the County roads, including paving the entire length of the Lincoln Highway in the County, and Arapahoe Road from Ten Mile Corner to the city.<sup>11</sup>

At the same time, another roads-related movement was gaining momentum around the country. In early 1919, shortly after the end of WWI, the American Forestry Association began promoting the idea of building “roads of remembrance” to “create one vast chain of Memorial Drives that will make the country easy to see and at the same time the most famous touring country

in the world.”<sup>12</sup> Editorial writers around the country took up the cause,<sup>13</sup> and soon plans for roads of remembrance were announced around America.<sup>14</sup>

One group that quickly warmed to the road of remembrance concept was the American Legion. Organized in 1919, the Legion’s membership at that time was limited to those who served honorably between April 6, 1917, and November 11, 1918.<sup>15</sup> In 1921, the Commander of the American Legion issued a ringing call to all state departments, urging them to push memorial tree planting everywhere. Shortly thereafter, he died in an auto accident, and soon embryonic Legion posts across the country began announcing plans to help fulfill his dream.<sup>16</sup>

Another development in 1919 was the appointment of Edward B. Hill, a player in local Republican politics, as Boulder County Commissioner for District 1.<sup>17</sup> Roads in unincorporated Boulder County were controlled by the Commissioners. Under the then-existing structure, the primary duty of a county commissioner was to maintain the roads in his district. The City of Boulder, Arapahoe Road and the Lincoln Highway south of Longmont were in District 1, under Hill’s jurisdiction.

Before long, news of the road of remembrance movement reached Boulder County.<sup>18</sup> In January 1923, the Boulder Commercial Association, a variety of the Boulder service clubs and the Boulder County Commissioners began working on the development of a “Road of Remembrance” on Arapahoe Road from Boulder to Ten Mile Corner, dedicated to the soldiers from the County who fought in WWI. Inspired by Commissioner Hill,<sup>19</sup> the preliminary plan included construction of an arch or monument at Ten Mile Corner, along with planting trees and possibly lights on both sides of the road.<sup>20</sup> Money for the arch would be raised by public subscription,<sup>21</sup> and most other organizations in Boulder endorsed the plan.<sup>22</sup>

Among the first Legion posts organized in Colorado was Boulder Post 10. The newly minted Post 10 quickly took up sponsorship of Hill’s “Road of Remembrance.”<sup>23</sup> By 1923, the outlines of a plan for a “Road of Remembrance” to connect with the Lincoln Highway had the full-throated support of both Boulder’s business community and civic organizations. However, politics, money and just plain stubbornness caused five years to pass before any element of the “Road of Remembrance” could be completed.

### **Relocation of the Lincoln Highway**

Though the Lincoln Highway was a burgeoning tourist thoroughfare in the summer, it was also the main north-south road in Boulder County and from northern Colorado to Denver. With the increase in travel and the higher speeds of newer cars, the dangers of the Highway’s zig-zag route from Ten Mile Corner to the beginning of the pavement south of Longmont became apparent. Four turns, including one named “Dead Man’s Curve,” made this stretch not only dangerous to tourists, but also a hazard to local residents.<sup>24</sup>

In November 1923, about nine months after the “Road of Remembrance” project was announced, the Boulder County Commissioners passed a resolution intended to eliminate these dangers by constructing a new, straight six-mile stretch of north-south highway. The new road would be built roughly three-fourths of a mile west of 111<sup>th</sup> Street, from Baseline Road west of Lafayette to Six Mile Corner (the intersection of 107<sup>th</sup> Street and Mineral Road) south of

Longmont. However, the proposed new highway did not follow existing roads, and would require obtaining land for a right of way from farmers along the route.<sup>25</sup>

Commissioner Hill championed the relocation of the Lincoln Highway, and by December announced that the plan was about to be approved by the State. He highlighted the dangers of the existing route, and emphasized that this plan would eliminate the dangers presented by the several sharp curves on that stretch of the Highway. A newspaper report of Hill's remarks added:

Mr. Hill stated that the owners of the farms through which the road must be built have expressed a willingness to grant a right-of-way and are anxious that the highway go over the proposed route. They show no inclination to try to prevent it and most of them have stated that they want no remuneration for the land but only ask that their places be left in as good a condition as they were before the road went through and that the expense of any building or fencing moving necessary be borne by the state and county.<sup>26</sup>

Hill's optimism was either overstated or nearsighted. The plan was supported by many Boulder residents, who were keen on moving this major thoroughfare incrementally closer to Boulder. But the proposal met with significant resistance from a variety of quarters elsewhere in the County. Some farmers, led by Joseph Oscar Vaughn (J.O.V.) Wise, vigorously protested. They maintained that the new road would ruin their farms and that the expense would be prohibitive. The opponents argued that the old route was adequate, and no changes were needed.<sup>27</sup>

After weeks of debate, the relocation project was put aside. However, it was revived in 1925 when State officials indicated that state funding for paving the Lafayette-Longmont connection would not be considered until the question was settled.<sup>28</sup>

Opponents again organized to block the relocation plan. In June 1925, the Lafayette Chamber of Commerce voted to protest the change, going so far as to call for court action seeking an injunction against the county commissioners if the project proceeded.<sup>29</sup>

Leaders of the Longmont Chamber of Commerce had attended the Lafayette meeting,<sup>30</sup> and also announced that it vigorously protested the proposed change.<sup>31</sup> And within a few days, a spat erupted with the Boulder Chamber. The ostensible flashpoint was the Boulder Chamber's distribution of 40,000 tourist folders with a map showing the natural wonders in the area. The Longmonters protested: "The aged or infirmed, without the aid of a powerful finding glass, could not possibly locate Longmont on the map used by Boulder..."<sup>32</sup>

The Longmont Chamber also saw other sinister moves afoot when Hill and the Boulder Chamber's "Boulder Good Roads Committee" met privately with the State Highway Commissioners in Denver. This meeting, the Longmonters argued, came on the heels of the prior summer when "Longmont and Northern Colorado was completely isolated to tourist travel with the lack of proper road improvement on detours, until the tourist season was all over."<sup>33</sup>

In late June it was announced that the Commissioners would meet with the resident state engineer, O.R. Douglas, to go over the proposed route for the relocated Lincoln Highway, and later explain the exact route to the affected farmers. While the other Commissioners remained silent, Hill reiterated his support for the plan.<sup>34</sup>

Despite these developments, Hill continued promoting the project and working to obtain the necessary right of way.<sup>35</sup> In early July 1925, he announced that he had spent the day with Douglas, going over the proposed right-of-way for the new route. Both men indicated that the owners of the land on the south end of the route had already signed up, but that opposition continued from farmers to the north. Hill indicated that he was continuing his efforts to secure the needed right-of-way, and expressed confidence that everything would be wrapped up by September of that year.<sup>36</sup>

However, led by Wise, four farmers who controlled about 20% of the route refused to sell.<sup>37</sup> According to some reports, the other two county commissioners were not in favor of buying the new right-of-way, but instead favored rebuilding the Highway along its existing route. Hill was undaunted, and told the recalcitrant farmers that if they did not agree, he would institute condemnation proceedings.<sup>38</sup>

Presumably because of the internal battles in Boulder County, the State stepped in. In September, the State Highway Engineer announced that the new paved Lincoln Highway would follow the route championed by Hill.<sup>39</sup> On October 2, 1925, the Colorado Attorney General, acting on behalf the Colorado Highway Commission, filed a condemnation action against the obstinate landowners in Boulder District Court.<sup>40</sup>

A month later in November 1925, the Boulder County Commissioners announced that if the condemnation was approved, Arapahoe Road would be paved from its intersection with the new highway to the City of Boulder.<sup>41</sup> This project, if completed, would provide the first paved connection between Boulder and Denver.

On December 23, 1925, a jury awarded the farmers slightly more than \$11,000 for the condemned land.<sup>42</sup> Initially, the County was hesitant to accept the verdict, and contemplated pursuing an alternative route.<sup>43</sup> However, the parties soon resolved their differences and the condemnation award was confirmed.<sup>44</sup>

### **Planning Boulder's Road of Remembrance**

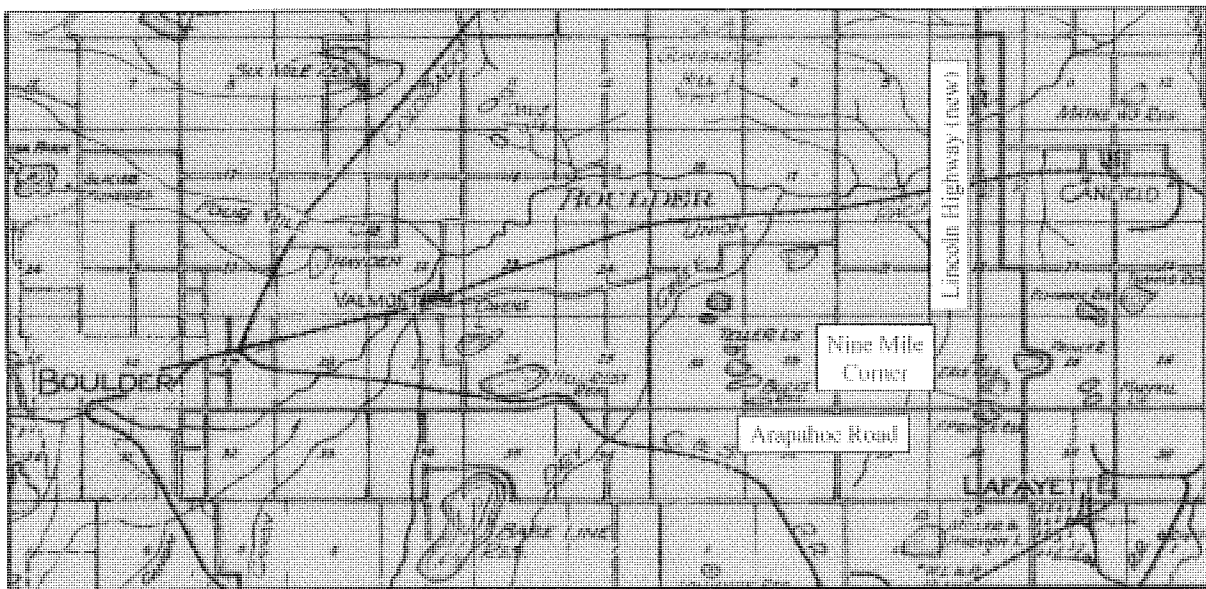
The fight over relocation of the Lincoln Highway put the plans for the "Road of Remembrance" along Arapahoe Road to Boulder on the back burner. The Legion had created a fund for the project in about 1924,<sup>45</sup> but no work had started.

While the Lincoln Highway fight raged, discussions continued about the potential Road. The original concept for the was ambitious. It called for the planting of over 1000 trees to line Arapahoe Road from Ten Mile Corner on the Lincoln Highway to Boulder. At every crossroads, grass and flowers would be planted forming miniature garden spots. At various other points, grassy parkways would be created. Several small lakes adjacent to the road would be incorporated into small parks with trees, benches, playgrounds, and row boats. Irrigation ditches would be constructed to maintain the plants. Adjacent farmers would be urged to construct attractive fences and maintain adjoining fields.<sup>46</sup>

At Ten Mile Corner, boosters continued to push for the creation of the "big arch" to entice tourists to turn toward Boulder.<sup>47</sup> The Boulder Lions Club soon took over that part of the project, and began to raise money to fund the arch's construction.

But the Legion soon added another element to the planning. In 1922, the parent American Legion began pushing Congress to release some of the nearly 2200 artillery pieces brought back as “souvenirs” by American forces returning from WWI. Local Legion posts were encouraged to lobby Congress for such trophies.<sup>48</sup> Shipment of the trophy guns began in the summer of 1925,<sup>49</sup> with Boulder’s Post 10 had receiving four captured artillery pieces.<sup>50</sup>

The relocation of the Lincoln Highway and the arrival of the war trophies opened new possibilities for the entrance way to Boulder on Arapahoe Road. Since the relocated stretch of Lincoln Highway was being built on an entirely new roadbed west of the old Highway, the new intersection would be “built from scratch.” No longer constrained by the limitations at the existing Ten Mile Corner, Hill, Douglas, and other enthusiasts began to plan a more eye-catching entrance at a spot that came to be known – for no particular cartographic reason – as “Nine Mile Corner.”<sup>51</sup>



*Nine Mile Corner* <sup>52</sup>

After the condemnation action was completed, the State and County in early 1926 began to disclose details for the design of the new Lincoln Highway. Douglas, the state’s resident engineer, was acutely aware of Boulder’s plan for Arapahoe Road.<sup>53</sup> Accordingly, its intersection with the relocated Lincoln Highway was designated to be “where the Legion Road of Remembrance will start its way west into Boulder.” The State resident engineer went on to state:

The end of Arapahoe road at this point will be spread into a wye with one arm curving north and the other curving south. The north and south road, however, will continue in a direct line, leaving a triangular park at the intersection. Here, according to tentative plans of the local Lions club and the American Legion, a memorial arch will be erected and captured German field pieces stationed.<sup>54</sup>

While the patriotic symbolism of this design was unmistakable, supporters pointed out another advantage:

The psychology back of the plan is that the casual motorist traveling the main highway will be instinctively attracted by the beautiful entrance to the road and that large numbers who would otherwise miss Boulder will turn their machine into the improved highway and follow it to the city.<sup>55</sup>

The enhanced connection to the Lincoln Highway was an increasingly important objective for the Boulder business community. In the mid-1920s, a push was underway for the federal government to identify routes along established roads across the country, and mark them with standardized number designations. The *cachet* from such official designations was expected to be boon to towns and cities on those routes.<sup>56</sup>

These designations were under discussion in 1925 and 1926, including a plan to designate a transcontinental route from El Paso to the Canadian border. This proposed route incorporated the north-south portion of the Lincoln Highway from Denver to the Wyoming border. Enthusiasts predicted that the new highway would attract millions of tourists each year to Colorado and Wyoming.<sup>57</sup>

Local leaders were thrilled by these developments. Boulder County Commissioner Guy Miller described the new stretch of highway as “part of an interstate road planned by the federal government ... [which] has been characterized as the most stupendous road building program now occupying the attention of federal road builders.”<sup>58</sup>

These few miles of road in eastern Boulder County were now, at least to local enthusiasts, part of both the legendary east-west Lincoln Highway and the soon-to-be-created north-south intercontinental federal highway. The push intensified to build the relocated Highway with an “instinctively attractive” entrance – a Gateway – to Boulder, in order to tap into the potential revenue from the increasing numbers of tourists northbound from Denver to Estes Park.

But yet another political dustup interfered. In early 1926, a battle between the Governor and the state highway department indefinitely tied up funding for road projects, including the relocation of the Lincoln Highway and the paving of Arapahoe Road.<sup>59</sup> Boulder’s dreams of a grand entrance beckoning to tourists traveling north once again were on hold.

### **The Battle at Ten Mile Corner**

The political wrangling in Denver could not have come at a worse time for the folks from Boulder. In January 1926, Hill trumpeted statistics indicating that nearly a half million autos had used the Lincoln Highway from June-September 1925, smashing previous records. Equally important for Boulder, approximately one-fourth of all of the annual traffic on the Lincoln Highway turned west on to Arapahoe Road toward the City.<sup>60</sup> With a pot of gold beckoning, Longmont and Boulder stepped up their competition. The focal point of the rivalry was Ten Mile Corner, the existing intersection of Arapahoe Road and the original Lincoln Highway.

The opening salvo was fired when the Boulder American Legion erected a large road sign near Ten Mile Corner. Precisely why the Legion was involved in the tourism business is unclear. Regardless, apparently dissatisfied with the results, the Legion next hung a banner across the Lincoln Highway reading “Boulder – Estes Park,” adorned with a large finger pointing west on Arapahoe Road.<sup>61</sup>



Heat was added to the competition when a small hut was erected at a cost of \$100 on the east side of the Lincoln Highway at Ten Mile Corner.<sup>62</sup> A large sign on the hut identified it as an information bureau and distribution station for road guides.<sup>63</sup> It was manned by “a rather genial but fluent talker” named David Fairchild, a Legionnaire who sought to persuade tourists to turn west on Arapahoe for Boulder.<sup>64</sup> According to multiple reports, Fairchild disparaged Longmont and points north, and promoted Boulder as the only town north of Denver worth seeing.<sup>65</sup>

In response, the Longmont Chamber of Commerce publicized that it had prepared a 10’ x 40’ road map showing the distances to all points of interest to be erected at Ten Mile Corner. The Longmont Chamber further boasted that it had acquired 100 new metal road signs to be erected at every crossroad north of Denver. Once erected, the Chamber crowed, no one will ever again have to ask, “Where is Longmont?”<sup>66</sup>

Apparently, Longmont’s crowing was an empty threat, and the signs never existed.<sup>67</sup> However, on the evening of July 16, 1926, the “Boulder information bureau” hut was destroyed by fire. A debate raged over the cause, with the parties variously blaming men from Longmont and Boulder.<sup>68</sup> One theory was that “it was an overabundance of Boulder ‘hot air’ which caused the conflagration.”<sup>69</sup> In any event, the hut was rebuilt by the Boulder American Legion, and Fairchild resumed his post.<sup>70</sup>

Though hard feelings continued for some time,<sup>71</sup> the Battle of Ten Mile Corner soon faded from view, as the completion of work on the new stretch of the Lincoln Highway moved the traffic west. Yet the energy directed at attracting tourists to Boulder from Ten Mile Corner, particularly by the Legion which was leading the “Road of Remembrance” project, corroborates the conclusion that patriotism was not the sole motive for the Boulder activists.

### **Construction of Nine Mile Corner**

Before construction on the Gateway could begin, the parties waited for the intersection with the new highway to be completed. After a year of infighting, the State finally approved the budget including the construction of the relocated Lincoln Highway.<sup>72</sup> Fifty percent of the funding for the new highway was provided by the federal government as part of Federal Aid Project Nos. 251 B and 281 D.<sup>73</sup>

The first order of business was acquisition of the land for the project. The four farmers who fought the highway had the rights of way on their land transferred to the State Highway Department.<sup>74</sup> With that accomplished, the remaining farmers who earlier had agreed to sell transferred their portions of the right-of-way to Boulder County.

As part of that acquisition, the County acquired additional land to accommodate the “arcs” (aka the north and south turn lanes) for the “wyé” forming the Gateway envisioned by Hill and Douglas at Nine Mile Corner. Because Arapahoe Road straddles the boundary line between Sections 27 and 34, two separate acquisitions were required. The southern half of the land required for the Gateway was part of the site of the Frank Prince Reservoir, and was acquired by the County from the Schofield Farms in April 1926.<sup>75</sup> The northern half of the land for the Gateway was purchased by the County from a farmer named Morris Spishakoff a month later in late May 1926.<sup>76</sup>

Grading for the new highway began in 1926 and was completed in 1927.<sup>77</sup> Paving at the intersection was completed in September 1927,<sup>78</sup> and Nine Mile Corner was ready for construction of the Gateway.

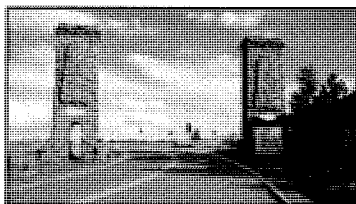
### **Building the Monument**

While work proceeded on the Highway, planning continued for the Gateway. Douglas had formally submitted his plan for roadway for the new Lincoln Highway in early March 1926. Hill triumphantly reported that this design included the “wyé” at Nine Mile Corner, to be built with both state and federal funds.<sup>79</sup>

As the plan for the Gateway evolved, the idea of a “big arch” was abandoned in favor of a somewhat different design:

The junction point will form the key to the whole beautification plan. Instead of a sharp right angle, enough land will be purchased to allow construction of sweeping curves from both north and south, thus making the turn easier to negotiate. The triangle which will be formed between these two arcs and the main highway will be made into a park and seeded with grass, shrubbery and flowers. Where the two arcs meet a stone gateway somewhat similar to that which guards the entrance to the Denver Mountain Parks above Golden will be erected. This particular phase of the project will be financed by the Lions Club of Boulder.<sup>80</sup>

Thus, the Gateway at the east end of the proposed “Road of Remembrance” would have two components: (a) a small park on the triangular tract formed by the roadways, and (b) a stone gateway at the apex of the two arcs financed by the Lions Club. The press reported that the “plan has been officially endorsed by most of the organizations in Boulder and it is expected that each will contribute to the financial outlay necessary to place it in effect.”<sup>81</sup>



*Lookout Mountain Park*

For their part, the Lions Club wanted its stone structure to “mark the new route” opened by the relocated Lincoln Highway,<sup>82</sup> as well as be a “suitable memorial in honor of those who served our country in the great World War.”<sup>83</sup> Loosely patterned after the pillars at the entrance to Lookout Mountain Park near Golden, the Monument would consist of two pillars flanking the roadway.

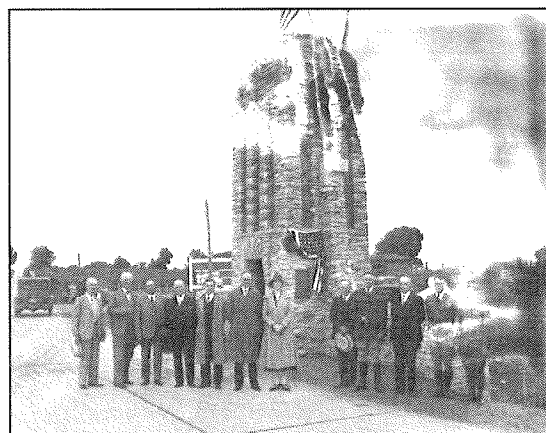
Plans for the Monument were drawn by Meade Walter, an architect and Lions Club member, whose son had served in the War.<sup>84</sup> The planned Monument would be built of flagstone, and “designed something like the walls and alcoves of the new University buildings.”<sup>85</sup> A three-man committee of Boulder Lions composed of Frank Henderson, Charles Cleveland and former Boulder Mayor James Brillig spearheaded the project.<sup>86</sup>

Precise original locations of the Monument pillars are presently unknown. However, the pillars as originally constructed flanked Arapahoe Road, which straddles the line between Sections 27 and 34. Accordingly, it is likely that the north pillar was originally constructed in Section 27 on land acquired by the County from Spishakoff. Similarly, it appears probable that the south pillar was built in Section 34 on land acquired by the County from Schofield Farms.

The groundbreaking and laying of the cornerstone for the Monument occurred on April 18, 1928. Approximately 200 officials and citizens gathered at Nine Mile Corner for the occasion. Joining Lions Club officials was Commissioner Hill, the godparent of the project. Members of the Legion and Legion Auxiliary, and representatives of the City of Boulder and the Boulder public schools attended. As part of the ceremony, a small metal box containing documents and mementos was placed by the Lions Club in a notch in the cornerstone.<sup>87</sup> No mention was made in the press reports of any attendees from Longmont or Lafayette.

Construction of the Monument took two months, with the stonework laid by Lee Roy Watson, a mason who also built some of the structures at St. Malo in western Boulder County.<sup>88</sup>

The Monument was dedicated on June 17, 1928, in a ceremony attended by more than 1,000 people, with music by the Boulder City Band and speeches from numerous dignitaries. The Dedication was described as “a beginning of the program that Boulder has adopted thru the Legion, of making Arapahoe road from the city limits to Nine Mile Corner, a Road of Remembrance.” During the ceremonies Cleveland, on behalf of the Lions, “formally presented the Gateway to the Legion.” City of Boulder Mayor, L.W. Cumberland, also “accepted the gateway in behalf of the city.”<sup>89</sup>



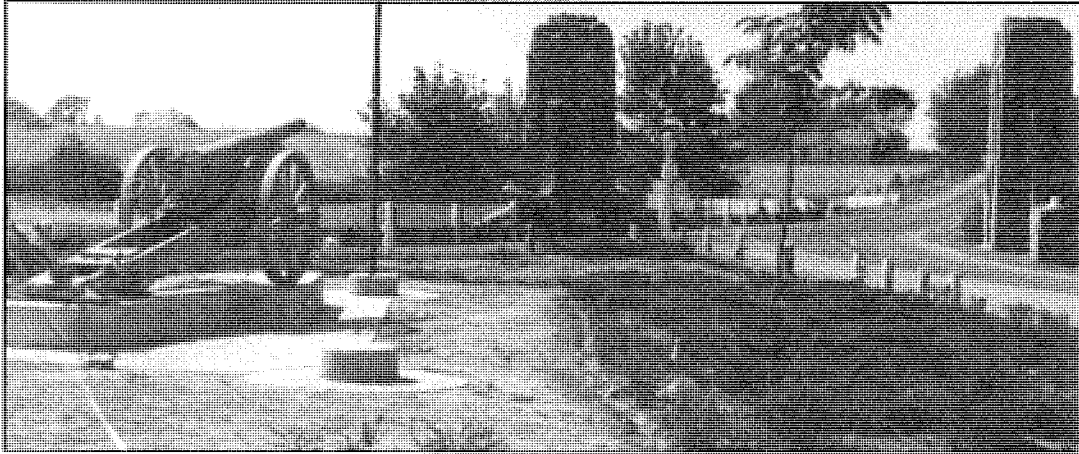
*Dedication June 17, 1928*

### **The Gateway Park**

While work was proceeding on the Monument, preparations were underway for construction of the second phase of the Gateway project, the triangular park immediately east of the Monument. By 1927, the Legion had raised over \$1000 for the project.<sup>90</sup> On December 30, 1927, Boulder County leased the ground for the park to the trustees for Boulder Post 10 for 99 years with rental payments of \$5.00 per year. The tract was described as “formed by the intersection of Federal Aid Project Road No. 251 B and Federal Aid Project Road No. 281 D.”<sup>91</sup>

A year after the Monument was completed, Boulder Post 10 built the park in the middle of the “wye” created by Douglas’ design. The Legion’s plan for the park centered on the captured artillery pieces received from United States government. Initially, it was unclear whether more than one of the guns would be placed in the park.<sup>92</sup>

On May 19, 1929, twenty-five Legion members bolted a single 100 mm captured German cannon<sup>93</sup> to a concrete base in the leased triangular tract of land.<sup>94</sup> The Legion also erected a donated 48’ flagpole, and arranged for a nearby neighbor raise and lower the American flag each day. The following week, they graveled the area.<sup>95</sup> Trees and shrubs were planted in the triangular plot, which was surrounded by barriers comprised of low posts and cables. The shoulder of the roadways of the arcs comprising the “wye” were sufficiently wide to permit parking for those accessing the park and the Monument.

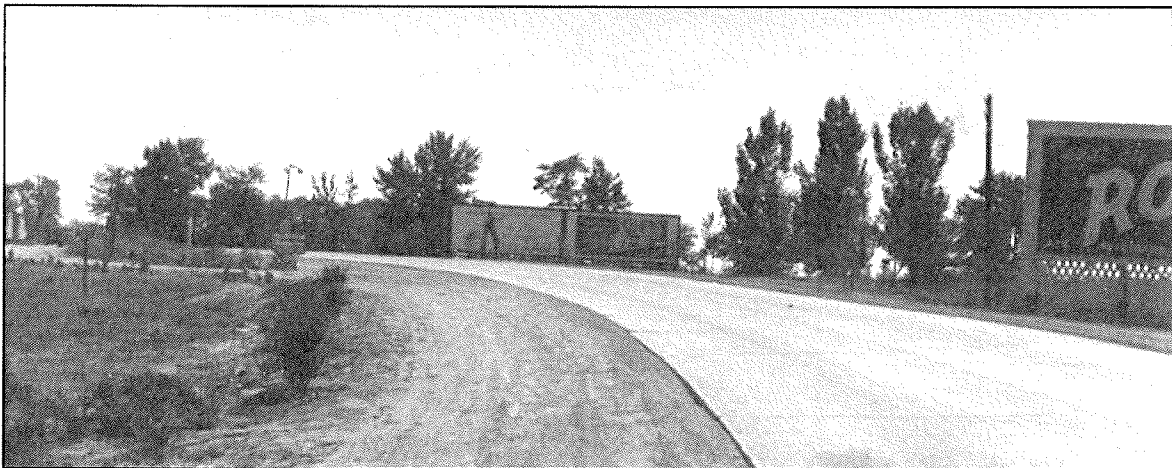


*Gateway, date unknown*<sup>96</sup>

### **The Decline of the Road of Remembrance**

Despite the noble sentiments espoused by the sponsors of the Gateway, the commercial competition between the various cities on the Lincoln Highway shifted from Ten Mile Corner to the new entrance to Boulder. Before the first shovel of dirt was turned for the Monument, Spishakoff, who had just sold land to the County for the north “arc” of the Gateway, began leasing land on his adjacent property near the intersection to out-of-state interests for the erection of “hideous and huge billboards.” Though the Boulder Lions protested, the County Commissioners admitted that they were helpless to stop the activity.<sup>97</sup>

Yet the folks from Boulder were hardly immune to commercialization at Nine Mile Corner. A primary motivation for the “beautification” of Nine Mile Corner was to entice tourists to turn west toward Boulder. Before the Monument was even built, plans were announced by the Boulder Chamber of Commerce in the spring of 1928 to erect their own 10’ x 40’ billboards at the intersection to direct tourists to Boulder. Other cities along the route threatened to do the same.<sup>98</sup> While it is unclear whether these billboards were ever built, Nine Mile Corner became home to numerous billboards.



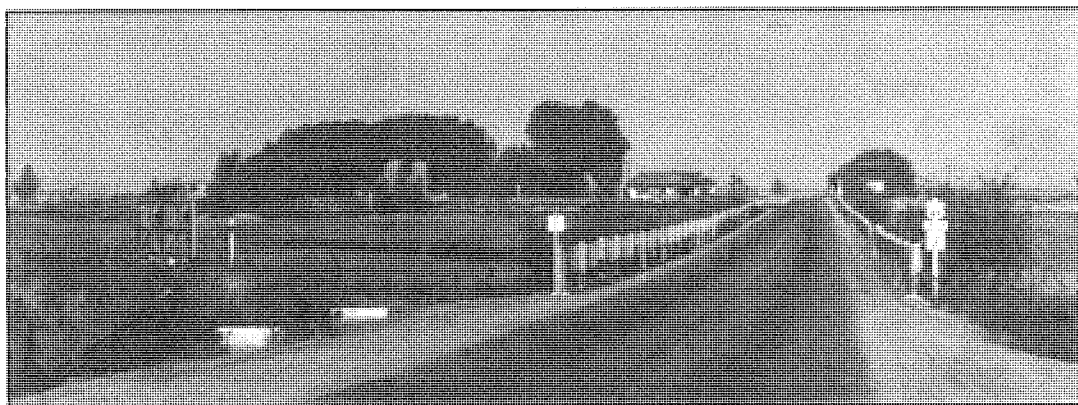
*Nine Mile Corner, date unknown*<sup>99</sup>

Later, to ensure that travelers did not miss the opportunity, three Boulder service clubs erected a large sign at the intersection unmistakably directing them to the city.



*Nine Mile Corner, date unknown*<sup>100</sup>

Much to the displeasure of the Boulder sponsors, commercialization of Nine Mile Corner continued. In 1928, Spishakoff sold a small parcel abutting the Lincoln Highway on the entrance to the northern arc to developers,<sup>101</sup> and a gas station and lunch stand operated by Lennie Herring were soon erected at that spot.<sup>102</sup> The Boulder Lions Club protested, contending that “it detracts much from the appearance of the beautiful gateway that they have erected.”<sup>103</sup>



*Nine Mile Corner, date unknown*<sup>104</sup>

With completion of the Gateway Park, the energy for the “Road of Remembrance” project among the various service clubs began to falter. The 1000 trees were never planted, though during the summer of 1928, Legion members planted hollyhock and poppy seed near streams and irrigation ditches near Arapahoe Road.<sup>105</sup>

Boulder County’s interest in the Road of Remembrance likewise began to wane. At the insistence of the United States Bureau of Roads, the state highway department had announced in October 1927 that the State, rather than the County, would maintain the Lincoln Highway across

the County, as well as Arapahoe Road to Boulder.<sup>106</sup> The latter in fact was designated as part of Colorado State Highway 7, running from Estes Park to Lafayette.<sup>107</sup>

Though the plaque on the north pillar stated that it was the “Road of Remembrance Gateway,” the Road remained merely a dream. Apart from the Monument and adjacent park, none of the proposed amenities for the Road were ever constructed. Nor has Arapahoe Road ever been known as the “Road of Remembrance.” While the onset of the Depression and attendant decline in tourism likely were contributing factors, Post 10’s and Boulder’s concept of creating a grand boulevard into the city never materialized.

### **Legion Park**

While never directly associated with the “Road of Remembrance,” Post 10 and the County also collaborated on another memorial project on Arapahoe Road east of the city. In 1917, long before the “Road of Remembrance” project was conceived, a local Boulder businessman, John Howard Empson, deeded 26 acres to Boulder County, including Goodview Hill on the north side of Arapahoe Road overlooking the Boulder Valley.<sup>108</sup> In 1931, Boulder County commissioners requested the construction of a new scenic road to the top of Goodview Hill to provide a vantage point from which visitors will get an “unparalleled panoramic view.”<sup>109</sup>

At that point, Post 10 proposed leasing the property “for the purpose of beautifying said tract as a memorial to the soldier dead of the World War.”<sup>110</sup> In 1932, the Post’s trustees signed another lease with the County, this time for the Goodview Hill property.<sup>111</sup>

The new Legion Park was designed by Saco Rienk DeBoer, a noted landscape architect from Denver. The design for the memorial called for 2’ high rock wall, 382 feet long, and 100 feet wide with flagpoles located at each end. Two more of the captured artillery pieces would be installed on platforms near the flagpoles. Notably, two elaborate entrance gateways were planned for the entrance road off Arapahoe Road.<sup>112</sup>

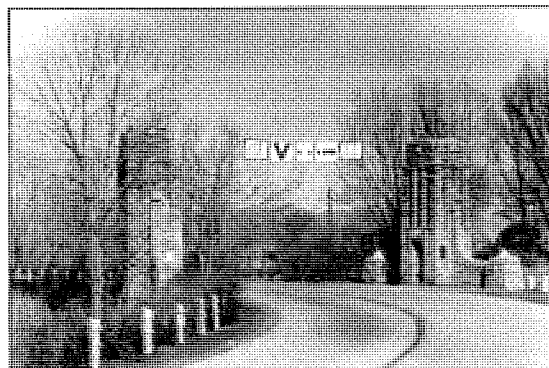
In January 1934, a crew from the Civilian Conservation Corps began construction. The memorial park with the commemorative cannons was completed, but the entrance gateways were never constructed. Legion Park was dedicated in late June 1934, and maintenance responsibilities were turned over to Legion Post 10. However, a later study concluded: “It is unclear what work, if any, occurred on the property after the American Legion signed the lease since the country was fast approaching the lowest point of the Great Depression.”<sup>113</sup>

### **The Withering of the Gateway**

Though the Road of Remembrance faded away, the Gateway remained a notable landmark in east Boulder County. For over two decades, the route east on Arapahoe Road and then south on the highway<sup>114</sup> was the primary access between Boulder and Denver. Boulder residents routinely passed between the Monument’s pillars on their way to and from the capital city.

While no records have been located concerning any maintenance on the Monument during the 1930s or 1940s, it was not forgotten by folks from Boulder. For example, the monument often was adorned with decorations during the Holiday season.<sup>115</sup> In 1942, the Boulder Chamber of Commerce purchased a new flag for the park, and arranged for Herring to care for it.<sup>116</sup>

During WWII, the Monument's patriotic heritage became more prominent. The pillars were used to promote the sale of war bonds, and display other patriotic materials to passers-by. When the County was awarded a flag in recognition of its great response to the Second War Bond Drive, the Governor suggested that rather than be displayed on Flagstaff Mountain, it fly at Nine Mile Corner due to the proximity to Longmont, Lafayette, Louisville, and Boulder.<sup>117</sup>

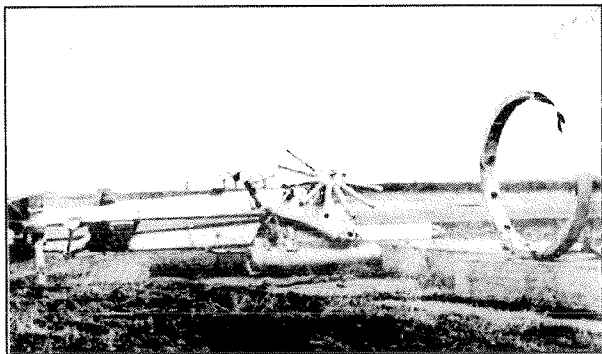


*Nine Mile Corner circa 1944*<sup>118</sup>

When WWII ended, Boulder's focus began to turn away from Nine Mile Corner. In January 1952, a four-lane toll road known as the Denver-Boulder Turnpike opened, connecting the two cities with a high speed, limited access expressway unlike anything in the state. The intercity route that began in 1928 as the Arapahoe Road – Lincoln Highway connection quickly slipped into irrelevancy for Boulderites. Tourists headed from Denver to Estes Park likewise no longer saw any need to pass through the Gateway to Boulder. The Gateway was no longer a “gateway,” but simply a relic of an earlier time.

Along with the Turnpike, the post-war population explosion in Boulder County began to impact the Gateway. In the mid-1960s, construction began on the Stonehenge subdivision in unincorporated Boulder County on Spishakoff's former land immediately northwest of the Monument.<sup>119</sup> It was soon followed by Arapahoe Ridge, an even larger development on the northeast corner of the intersection that was annexed by the Town of Erie. On the south and west, annexations by Lafayette led to developments that crept closer and closer to Nine Mile Corner. Though it remained in the unincorporated County, the Gateway no longer was out on the prairie, miles from any town.

During this time, the park at the Gateway was falling into disrepair. No record has been located suggesting that Post 10 undertook any maintenance or repair at Nine Mile Corner. Jim Hutchison, a Lafayette historian, observed that over time the cannon and flagpole were “hammered into semiruin by cars of miscalculating motorists.”<sup>120</sup>



*Cannon at Gateway Park, February 19, 1972*<sup>121</sup>

On February 19, 1972, a speeding car struck and badly damaged the cannon. The Legion indicated that the gun would be placed on its axles at the Legion Home in Boulder as soon as the Post obtained equipment to move it. State highway officials indicated that they once wanted the gun removed, but now relocation would not be required until CO 7 (Arapahoe Road) was improved in another six to eight years.<sup>122</sup> Apparently, the gun was removed at that time, but the concrete platform remained.

At the same time, Legion Park was falling into significant disrepair. In early 1976, due to the county's concern of the ability of Post 10 to properly maintain the park, Boulder County Parks and Open Space Department took control of the park's maintenance, though the Legion continued its sponsorship of the memorial. Toward that end, the County signed a Memorandum of Agreement with the Legion re-executing their March 1, 1933 lease, and outlining that the County would take responsibility for the care of the park.<sup>123</sup>

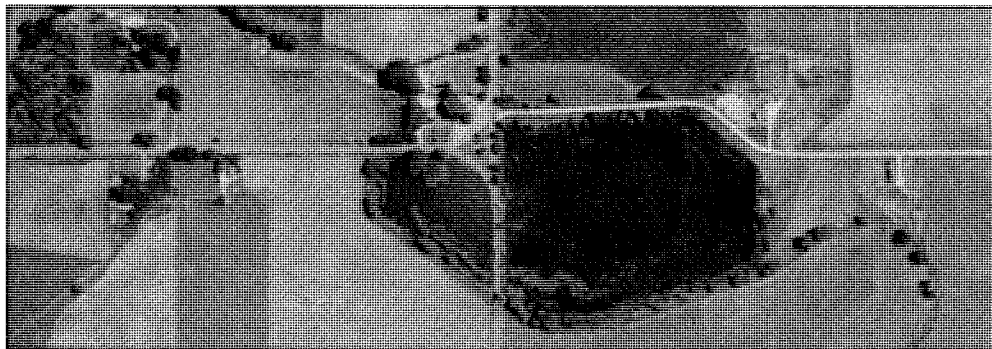
Nothing in that document mentioned the Monument at Nine Mile Corner. Nor has any legal instrument regarding control of the Monument, similar to the leases for the two parks, been located. Likewise, no record has been found reflecting any maintenance of the Monument by the County, the State, the Legion, or any other party from 1928 until 1983.

In May 1980, the Monument was part of a Colorado Cultural Resource Survey prepared by the Boulder County Historical Society. The Monument's owner was listed as "The Lions Club of Boulder." The Survey indicated that the Monument was on its original site, had not been altered and was in good condition, though the stone "may need to be repointed in the near future."<sup>124</sup> The Monument was not shown as having any architectural significance, but was listed as having historical significance as "associated with significant events or patterns."<sup>125</sup> Accordingly, the Monument was considered "field eligible" to be listed on the National Register of Historic Places, though no official determination has been made by the State Historic Preservation Office.<sup>126</sup>

### **The Widening of Arapahoe Road in 1983**

When the Gateway park was completed in 1929, Boulder County's population was roughly 32,000. By 1980, the number of residents had grown to nearly 190,000, nearly a six-fold increase. Though Nine Mile Corner had ceased to be part of the main thoroughfare between Boulder and Denver, it was used as a local artery by the increasing numbers of suburbanites in the East County. The time for "modernizing" the intersection had come.

As reflected above (see map, p. 6), Arapahoe Road east of the new Lincoln Highway veered slightly northward to avoid the Prince Reservoir. When he designed the "wye," Douglas oriented the northern arc to align with the continuation of Arapahoe Road east of Nine Mile Corner.



*Aerial view of Nine Mile Corner in 1949*<sup>127</sup>

In the early 1980s, a decision was made to change the configuration of the intersection, *inter alia*, by shifting Arapahoe Road on the east side of the intersection to the south to more

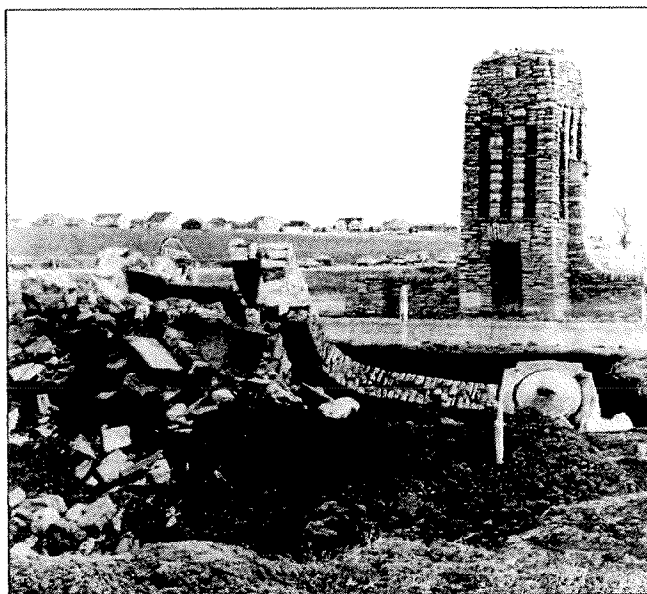


directly align with Arapahoe Road (CO 7) on the west side, and widening Arapahoe Road west of the intersection. The project was identified by CDOT as Project No. FC-HES 007-2. In preparation for that project, the Town of Erie executed a Warranty Deed in February 1983 transferring to the State several small parcels to comprise part of the expanded right of way for Arapahoe Road on the west side of US 287.<sup>128</sup> Additional land for the right of way was acquired by the State from other landowners.<sup>129</sup>

As part of the project, the Gateway was substantially modified by (a) removing and paving over the remaining vestiges of the triangular park, and (b) moving the north pillar of the Monument further north to allow for the widening of Arapahoe Road.<sup>130</sup> No record has been located reflecting the deliberations underlying that decision. Research indicates that a study (not yet located) was prepared for the project by the Colorado Department of Highways entitled "Project Fc-Hes 007-2(s), S.H. 7 and U.S. 287; Cultural Resource Report for Historical Resources,"<sup>131</sup> which presumably discusses those issues.

Flatiron Paving Company was hired by the State to perform the work on Arapahoe Road in 1983. During that construction work, the remnants of the Gateway park were demolished by Flatiron to make way for the widened road. With regard to the Monument, Flatiron in April 1983 subcontracted with Whitlock House Moving of Louisville to move the north pillar 40 feet further north.<sup>132</sup>

Prior to moving the Monument, a "methods statement" reportedly was submitted to the state highway department and the State Historical Society, though that document has not been located. According to James Combs from the state highway department, the plan involved digging into the sides of the north pillar, jacking it up, putting steel beams beneath it, and then sliding it to the new location.<sup>133</sup>



*North Pillar, April 1983*<sup>134</sup>

A witness described what occurred when Whitlock attempted to move the pillar: "This old, wet sandy soil just gave way. It took about 10 minutes for it to fall over. It leaned over a little at a time, and the bank was caving in. Then when it hit, it crumbled to pieces. It's too bad."<sup>135</sup>

Flatiron agreed to rebuild the Monument, with a spokesman commenting: "We've got to construct the monument back to what it originally looked like. It's hard to tell at this point what the cost'll be. I guess if it wasn't a historical monument we could get by a lot cheaper."<sup>136</sup>

The reconstruction plan involved cleaning the unbroken flagstone, and replacing the broken stones with flagstone of matching color. Since Walter's original written plans were not available, the rebuilt pillar was modelled after the still-standing south pillar, using similar techniques as the original.<sup>137</sup>

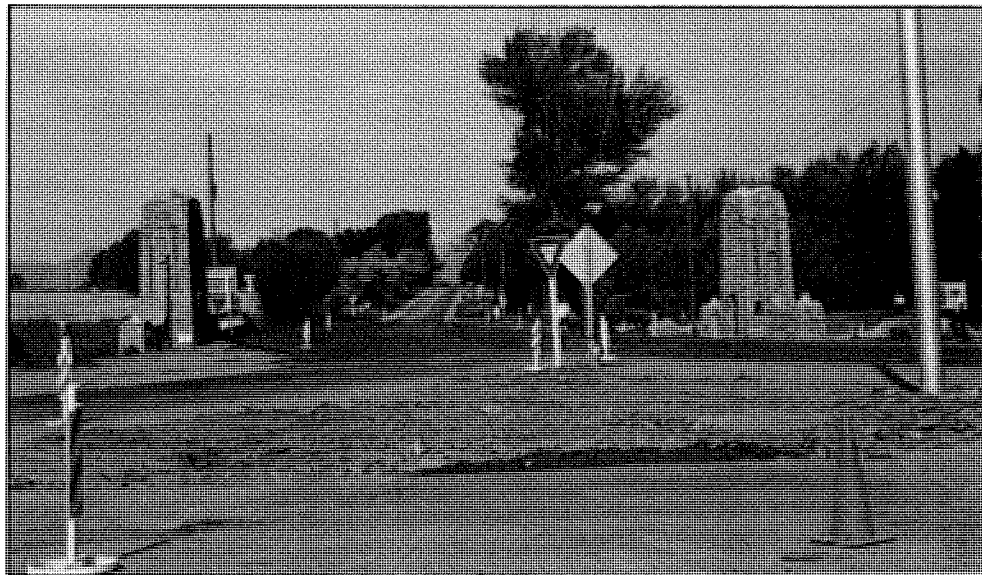
Flatiron Paving hired Del's Masonry of Longmont to rebuild the pillar. Del's assigned Lorri and Roy Martinez to do the work. The new pillar was rebuilt approximately 40' north of the location of the original north pillar. The Martinezes completed the work in the summer 1983, using about 80% of the original stones.<sup>138</sup> The restoration work took several weeks, and cost roughly \$20,000.<sup>139</sup>



*Rebuilding the north pillar, July 1983*<sup>140</sup>

Apart from press reports, no documentation from the 1983 project regarding the Monument has been located. Nor has any survey data been discovered that gives the precise location of the rebuilt north pillar. Based on the best available information, it appears that the north pillar was rebuilt – and currently remains – on land acquired for the widening project by the State in 1983 from a private landowner.<sup>141</sup>

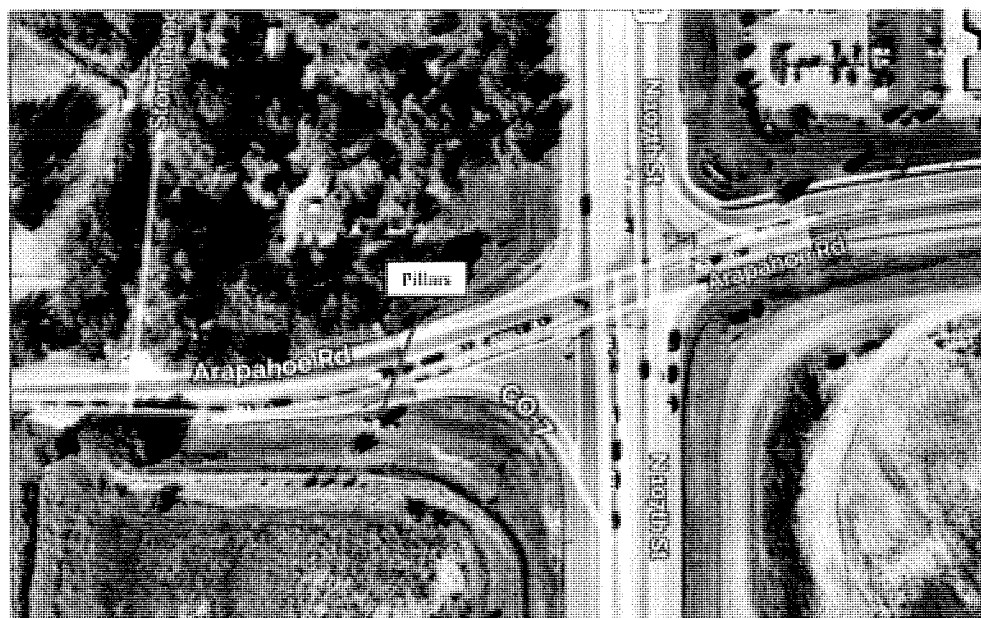
After the north pillar was rebuilt, the spacing between the pillars was significantly increased to allow for the additional traffic lanes.



*1983 Widening and Reconstruction*<sup>142</sup>

With the completion of the 1983 reconstruction of the intersection, the Gateway as planned and constructed in 1928 had been largely demolished. The two sweeping arcs of the roadway joining at the Monument were replaced by a significantly widened single ribbon of concrete. The

park, cannon and flagpole were gone, covered over with hardscape. While it appears that the south pillar remained in its original location, the north pillar had been destroyed and rebuilt 40' from its original location.



*Nine Mile Corner 2020*

### **The 2021 Reconstruction of the Intersection**

At this time, no documentation has been located to suggest that any significant additional construction occurred near the Monument between 1983 and 2021. However, in 2021, a new project again impacted the remaining portions of the Monument.

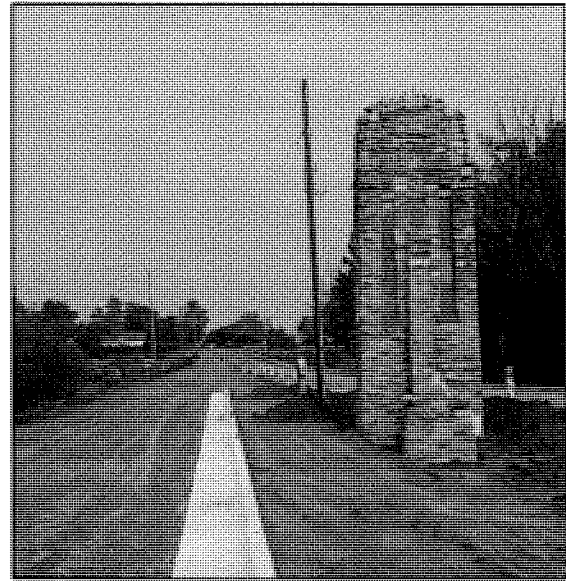
The project, known as the Nine Mile Corner Development, lay on the southeast corner of the intersection. In 2015, the Town of Erie announced a public-private partnership with Evergreen Devco, Inc. to develop the 45-acre tract. As part of the development, it was decided to add lanes to Arapahoe Road and reconfigure its intersection with US 287.

Those planning the new development were aware of the potential impacts of the project on the Monument. On October 20, 2015, Tyler Carlson, a principal at Devco, publicly stated that the construction would “complement the existing historical architecture” and “honor the 1928 vision of beautifying the intersection where the historic Road of Remembrance Gateway still stands.”<sup>143</sup>

For several years, the development was delayed by litigation between Erie and Lafayette. These disputes eventually were resolved, and Evergreen retained Galloway & Company, Inc. to design the improvements to the intersection, including on the west side on Arapahoe Road. According to Galloway, it “collaborated closely with” the Erie, Lafayette, the Colorado Department of Transportation (CDOT), and Boulder County in developing its plan.<sup>144</sup> Because Arapahoe Road west of the intersection is a state highway, the primary jurisdiction for approval of the design and completion of the work in the vicinity of the Monument lay with the CDOT.

The plan prepared by Galloway involved adding another eastbound through traffic lane on the west side of the intersection. To make room for that lane, the eastbound right turn lane was relocated, so that it passed south of the south pillar of the Monument.

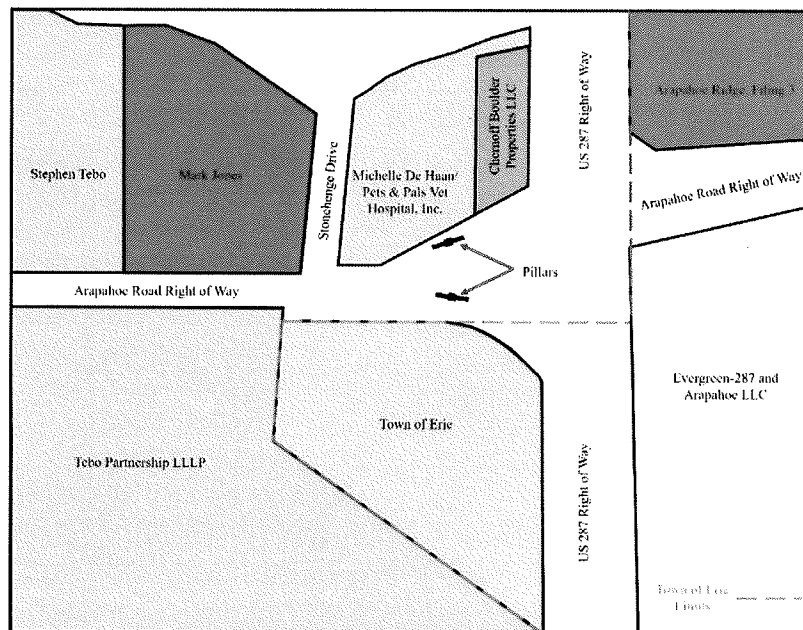
During this construction, the south pillar was not physically altered. Instead, it was encircled by a raised “bump out” traffic island covered with hardscape, and protected by guard rails. The north pillar of the Monument was untouched by this project, and remains where it was rebuilt on the north shoulder of Arapahoe Road/CO 7 in 1983. Both are in significant disrepair with crumbling masonry, deteriorating concrete ornamentation, and broken windows.



*Intersection on August 1, 2021*

### **The Future**

Development in the area is continuing, and perhaps accelerating. At this writing, Lafayette has annexed and approved a new urban mixed-use community with 473 dwelling units plus nearly 30,000 ft<sup>2</sup> of office/commercial space, which is under construction on Arapahoe Road a few hundred feet west of the Monument.<sup>145</sup> Additional developments are in the planning stages immediately to the south and west of the Monument.



*Land Ownership 2021*<sup>146</sup>

At the same time, Arapahoe Road/State Highway 7 has become “a critical east-west arterial in the regional transportation system serving Boulder, Lafayette, Erie, unincorporated Boulder County, and the north Denver metropolitan area.”<sup>147</sup> With the ever-increasing population in the area, coupled with the prospect of future development in the vicinity, it appears likely that transportation authorities will undertake additional construction/reconfiguration of the intersection at Nine Mile Corner within the next twenty years.

William D. Meyer  
Boulder Rotary Club  
Revised September 27, 2021

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<sup>1</sup> The Lincoln Highway Association, *The Lincoln Highway: The Story of a Crusade that Made Transportation History*, p. 60, New York: Dodd & Mead, 1935; Wolfe, Mark, “How the Lincoln Highway Snubbed Colorado,” *Colorado Heritage*, p. 3, (Autumn 1999).

<sup>2</sup> See “Proclamation of route of The Lincoln Highway,” found at [https://en.wikisource.org/wiki/Proclamation\\_of\\_route\\_of\\_The\\_Lincoln\\_Highway](https://en.wikisource.org/wiki/Proclamation_of_route_of_The_Lincoln_Highway) [accessed August 8, 2021].

<sup>3</sup> *Ibid.*

<sup>4</sup> “Colorado Protesting,” *The Daily Deadwood Pioneer-Times*, March 21, 1915; *The Lincoln Highway: The Story of a Crusade that Made Transportation History*, p. 158; Wolfe, p. 3.

<sup>5</sup> See *The Complete Official Road Guide of the Lincoln Highway*, Second Edition, p. 117, Detroit: The Lincoln Highway Association, 1916 .

<sup>6</sup> Wolfe, *supra*.

<sup>7</sup> Connarroe, Doug, “For two brief years, the (official) Lincoln Highway came through Lafayette,” September 4, 2019, found at <https://www.lafayettehistory.com/the-two-brief-years-that-the-official-lincoln-highway-came-through-lafayette/> [accessed August 5, 2021]. See, e.g., “Commissioners To Over New Route of Highway Next Week,” *The Daily Times*, June 26, 1925.

<sup>8</sup> Original map titled “Road and trail map, Boulder County, Colorado,” drawn by Art Boase, circa 1920, located in the Jerry Crail Johnson Earth Sciences & Map Library, University of Colorado (“Boase Map”).

<sup>9</sup> Original from Boase Map.

<sup>10</sup> Letter dated September 27, 1919 from David H. Holmes to George P. Schumaker, found in “Colorado Roads Scrapbook 1916-1920,” Call No. BHS 300-2-11, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>11</sup> “Roads from Lyons to Boulder and Longmont to be paved,” *The Lyons Recorder*, Vol. XIX, No. 31 (July 10, 1920); “Road Enthusiasts Outline Campaign,” *Fort Collins Coloradan*, December 10, 1920; letters dated November 26, 1920 from Harry E. Niven to David H. Holmes, and January 14, 1921 from David H. Holmes to Wm. G. Evans, found in “Colorado Roads Scrapbook 1916-1920,” Call #BHS 300-2-11, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>12</sup> “Charles L. Pack Suggests Highways Instead of Stone Monuments,” *Washington Post*, p. 30, June 15, 1919; “Memorial Trees,” *American Forestry*, Vol XXV, No. 307, p. 1201 (July 1919); *id.*, “For Them a Tree Stands,” p. 1270.

<sup>13</sup> “Editors Take Up Forest Matters,” *American Forestry*, Vol XXV, No. 307, p. 1296 (July 1919).

<sup>14</sup> “Roads of Remembrance,” *American Forestry*, Vol XXV, No. 309, p. 1334 (September 1919).

<sup>15</sup> See Wheat, George Seay, *The Story of the American Legion*, New York: G. P. Putnam’s Sons, 1919.

<sup>16</sup> “Legion to Plant Trees,” *Los Angeles Herald*, Vol. XLVI, No. 281 (September 24, 1921).

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<sup>17</sup> *Lyons Recorder*, Vol. XIX, No. 19 (October 18, 1919).

<sup>18</sup> "Memory Trees to Soldiers," *Longmont Ledger* (July 1, 1921).

<sup>19</sup> Clipping dated September 7, 1927 titled "Chauncey Vivian Tells the World of Boulder's Remembrance Road," ("Vivian Article") located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado. Vivian was the commander of Boulder Legion Post 10 in the 1920s.

<sup>20</sup> "Lions Outlined Plans to Help Clubs of City and Chamber in Their Co-Operative Efforts," *Boulder Daily Camera*, January 17, 1923, located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>21</sup> "County Will Improve Base Line Road," *The Lafayette Leader*, Vol. XX, No 4 (January 26, 1923).

<sup>22</sup> Vivian Article.

<sup>23</sup> Vivian Article.

<sup>24</sup> See, e.g., "Auto Overturns, Five Are Hurt," *The Longmont Call*, Vol. XVIII, No. 15 (December 17, 1915); "'Bob' Snow Somersaults In Morgan Ford Truck," *The Lafayette Leader*, Vol. XV, No 50, (December 13, 1918).

<sup>25</sup> "Lafayette People Opposed to Change in Highway Route and Threaten to Bring Injunction," *The Daily Times*, Vol. XXXI, No. 153 (June 11, 1925).

<sup>26</sup> "Tentative Plans Made for Paving," *The Lafayette Leader*, Vol. XX, No 50 (December 14, 1923).

<sup>27</sup> "Lafayette People Opposed to Change in Highway Route and Threaten to Bring Injunction," *The Daily Times*, Vol. XXXI, No. 153 (June 11, 1925).

<sup>28</sup> *Ibid.*

<sup>29</sup> *Ibid.*

<sup>30</sup> *Ibid.*

<sup>31</sup> "Farmers on Proposed Highway Route Refuse to Sell County Right-of-Way," *The Lafayette Leader*, Vol. XXII, No 30 (July 24, 1925).

<sup>32</sup> "That Break between Longmont & Boulder," *The Daily Times*, Vol. XXXI, No. 163 (June 23, 1925).

<sup>33</sup> *Ibid.*

<sup>34</sup> "Commissioners Go Over New Route of Highway Next Week," *The Daily Times*, Vol. XXXI, No. 166 (June 26, 1925).

<sup>35</sup> *Ibid.*; "Farmers Signing Up for New Right of Way South of Longmont," *The Daily Times*, Vol. XXXI, No. 174 (July 7, 1925).

<sup>36</sup> "Farmers Signing Up for New Right of Way South of Longmont," *The Daily Times*, Vol. XXXI, No. 174 (July 7, 1925).

<sup>37</sup> "State Highway Engineer Decides on West Route for New Paved Road," *The Lafayette Leader*, Vol. XXII, No 37 (September 11, 1925).

<sup>38</sup> "Farmers on Proposed Highway Route Refuse to Sell County Right-of-Way," *The Lafayette Leader*, Vol. XXII, No 30 (July 24, 1925).

<sup>39</sup> "State Highway Engineer Decides on West Route for New Paved Road," *The Lafayette Leader*, Vol. XXII, No 37 (September 11, 1925).

<sup>40</sup> "State Files Suit to Build New Highway," *The Daily Times*, Vol. XXXI, No. 247 (October 3, 1925).

<sup>41</sup> "State to Pave Longmont Road if Lafayette Stretch is OK," *The Daily Times*, Vol. XXXI, No. 276 (November 2, 1925).

<sup>42</sup> "Jury Gives Verdict in Highway Case," *The Daily Times*, Vol. XXXII, No. 8 (December 24, 1925).

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<sup>43</sup> “Right-of-Way Question is Expected to be Paramount One at Meeting of Advisory Board of State Next Week,” *The Daily Times*, Vol. XXXII, No. 25 (January 15, 1926).

<sup>44</sup> “Farmers to Dismiss Road Case Monday Boulder Newspaper Says,” *The Daily Times*, Vol. XXXII, No. 38 (January 3, 1926).

<sup>45</sup> Vivian Article.

<sup>46</sup> *Ibid.*

<sup>47</sup> “County Will Improve Base Line Road,” *The Lafayette Leader*, Vol. XX, No 4 (January 26, 1923).

<sup>48</sup> “Booty from the Boche,” *The American Legion Weekly*, Vol. 4, p. 11 (September 8, 1922).

<sup>49</sup> “Parade to Escort Trophies for Lodge on Lookout,” *Jefferson County Republican*, Vol. 6, No. 36 (July 30, 1925).

<sup>50</sup> Legion Park Landmark Nomination Form (undated), prepared by Boulder County Land Use Department, attached to Agenda for Boulder County Historic Preservation Advisory Board, September 5, 2019 (“Legion Park Nomination Form”).

<sup>51</sup> In the early years, the spot was also known as “the new Ten Mile Corner” or “Four Mile Corner,” the latter presumably based on the distance from Lafayette.

<sup>52</sup> Boase Map.

<sup>53</sup> “Plans for New Road Submitted by Local Engineer to State,” *The Daily Times*, Vol. XXXII, No. 65 (March 3, 1926).

<sup>54</sup> “Plans Nearing Completion for Right of Way Highway; Local Engineer Working on New Route,” *The Daily Times*, Vol. XXXII, No. 60 (February 25, 1926).

<sup>55</sup> Vivian Article.

<sup>56</sup> See Weingroff, Richard F., “From Names to Numbers: The Origins of the U.S. Numbered Highway System,” Federal Highway Administration (June 27, 2017), found at <https://www.fhwa.dot.gov/infrastructure/numbers.cfm> [accessed August 17, 2021].

<sup>57</sup> “World’s Greatest Road May Be Built Through Longmont,” *The Daily Times*, Vol. XXXII, No. 58 (February 23, 1926).

<sup>58</sup> “Plans Nearing Completion for Right of Way Highway; Local Engineer Working on New Route,” *The Daily Times*, Vol. XXXII, No. 60 (February 25, 1926).

<sup>59</sup> “Right-of-Way Damages Expected by Commissioners in Next Week,” *The Daily Times*, Vol. XXXII, No. 44 (February 6, 1926). See also “Governor May Ask Supreme Court Today to Untangle Hiway Mixup,” *The Daily Times*, Vol. XXXII, No. 30 (January 21, 1926).

<sup>60</sup> “One Million Autos Annually Use Lincoln Highway,” *The Daily Times*, Vol. XXXII, No. 35 (September 27, 1926).

<sup>61</sup> “‘Right This Way to Estes Park,’ Cries Boulder Man Stationed in Hut at 10-mile Road Intersection,” *The Daily Times*, Vol. XXXII, No. 172 (July 8, 1926); clipping titled “Longmont Editor Objects To Plan Of Boulder To Get Some Tourists,” *Boulder Daily Camera* (July 9, 1926), located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>62</sup> “Boulder ‘Steering’ Hut Burns Friday Night,” *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926); clipping titled “Legion Information Booth at Ten-Mile Corner Mysteriously Burned to Ground Last Night; Incendiarism Is Suspected,” *Boulder Daily Camera* (July 17, 1926), located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>63</sup> “Boulder ‘Steering’ Hut Burns Friday Night,” *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>64</sup> “Boulder Tourist Hut at Ten Mile Corner Burned,” *The Daily Times*, Vol. XXXII, No. 180 (July 17, 1926). See also “Water Flow in River,” *The Daily Times*, Vol. XXXI, No. 159 (June 18, 1925).

<sup>65</sup> “‘Right This Way to Estes Park,’ Cries Boulder Man Stationed in Hut at 10-mile Road Intersection,” *The Daily Times*, Vol. XXXII, No. 172 (July 8, 1926); “Boulder Paper Sees Unfairness of ‘Man in Hut’ at Intersection,” *The*

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*Daily Times*, Vol. XXXII, No. 179 (July 16, 1926); "Boulder 'Steering' Hut Burns Friday Night," *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>66</sup> "New Longmont Road Sign Will Properly Direct All Tourists to Destination," *The Daily Times*, Vol. XXXII, No. 174 (July 10, 1926).

<sup>67</sup> See Connarroe, Doug, "The battle over Ten Mile Corner and how Nine Mile Corner in Erie got its name," September 11, 2019, found at <https://www.lafayettehistory.com/how-nine-mile-corner-in-erie-got-its-name/> [accessed June 21, 2021].

<sup>68</sup> "Boulder Tourist Hut at Ten Mile Corner Burned," *The Daily Times*, Vol. XXXII, No. 180 (July 17, 1926); "Comments: The Tourist Bureau at Ten-Mile Corner," *The Daily Times*, Vol. XXXII, No. 181 (July 19, 1926); clipping titled "Legion Information Booth at Ten-Mile Corner Mysteriously Burned to Ground Last Night; Incendiarism Is Suspected," *Boulder Daily Camera* (July 17, 1926), located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>69</sup> "Boulder 'Steering' Hut Burns Friday Night," *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>70</sup> "Comments: The Tourist Bureau at Ten-Mile Corner," *Daily Times*, Vol. XXXII, No. 181 (July 19, 1926); "Boulder 'Steering' Hut Burns Friday Night," *The Lafayette Leader*, Vol. XIV, No. 30 (July 23, 1926).

<sup>71</sup> See "Boulder-Longmont Joint Program Armistice Day Will Not Be held," *The Daily Times*, Vol. XXXII, No. 240 (September 25, 1926).

<sup>72</sup> "Longmont-Lafayette Road Will Be Completed As Budget Is Accepted," *The Daily Times*, Vol. XXXIII, No. 21 (January 7, 1927).

<sup>73</sup> "Three Paving Districts Will Connect Links Thru Lafayette North," *The Daily Times*, Vol. XXXII, No. 111 (April 26, 1926). See also Lease recorded December 30, 1927 at Book 559, p. 139 in the records of the Boulder County Clerk and Recorder.

<sup>74</sup> Judgment and Decree recorded on February 11, 1926 at Book 530, p. 372 in the records of the Boulder County Clerk and Recorder.

<sup>75</sup> Deed recorded November 26, 1926 at Book 543, p. 519 in the records of the Boulder County Clerk and Recorder.

<sup>76</sup> Deed recorded May 26, 1926 at Book 543, p. 52 in the records of the Boulder County Clerk and Recorder.

<sup>77</sup> "Grading and Drainage Bids Will Be Opened June 18," *The Lafayette Leader*, Vol. XIV, No. 24 (June 11, 1926).

<sup>78</sup> "Pouring of Concrete on 6-Mile Stretch South of City Started by Miller," *The Daily Times*, Vol. XXXIII, No. 243 (September 24, 1927).

<sup>79</sup> "Plans for New Road Submitted by Local Engineer to State," *The Daily Times*, Vol. XXXII, No. 65 (March 3, 1926).

<sup>80</sup> Vivian Article.

<sup>81</sup> *Ibid.*

<sup>82</sup> Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 20, 1980, obtained from the records of the Boulder County Planning Department.

<sup>83</sup> Clipping dated April 11, 1928, titled "Boulder Lions Club to Lay Cornerstone for New Gate Wednesday," located in folder 328-52-20 maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>84</sup> *Ibid.*

<sup>85</sup> Undated clipping titled "Lions Club Breaks Ground for Memorial Gateway East of City," located in folder 328-52-20 maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>86</sup> Clipping titled "Lions Club Paid its Respects to Those Who Served in War," *The Boulder Daily Camera*, June 18, 1928, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>87</sup> Clippings titled "Boulder Lions Club To Lay Cornerstone For New Gate Wednesday," dated April 11, 1928; "Cornerstone for Boulder Lions Memorial Gateway Will Be Laid In Impressive Services," dated April 17, 1928;



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"Lions Club Breaks Ground for Memorial Gateway East of City," dated April 18, 1928; "Speakers Dedicate Lions Club Gateway to Memory of Boulder World War Dead," dated April 18, 1928, located in folder 328-52-20 maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>88</sup> Information from Gail Elias, Regent, Indian Peaks Chapter, NSDAR.

<sup>89</sup> Clipping titled "Lions Club Paid its Respects to Those Who Served in War," *The Boulder Daily Camera*, June 18, 1928, maintained by the Carnegie Library for Local History, Boulder, Colorado; "Boulder Gate Is Dedicated," *Fort Collins Express-Courier*, (June 18, 1928). See also undated pamphlet titled "Greetings from the Lions in the Den," located in the Lions Club Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>90</sup> Vivian Article.

<sup>91</sup> Lease recorded December 30, 1927 at Book 559, p. 139 in the records of the Boulder County Clerk and Recorder.

<sup>92</sup> Clipping titled "Lions Club Paid its Respects to Those Who Served in War," *The Boulder Daily Camera*, June 18, 1928, maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>93</sup> In press reports, there was confusion over which of the guns was installed at Nine Mile Corner. When that gun was destroyed in 1972, one press report referred to it as a 75 mm gun. Clipping dated March 13, 1972 from the *Longmont Times-Call* titled "Direct Hit Means Removal," provided by the Longmont Museum. However, other reports and photographs confirm that the gun at Nine Mile Corner was the 100 mm gun received by Post 10. The captured 77 mm gun was initially installed at Legion Park, and later restored and moved to Post 10's facility on North Broadway in Boulder. See Legion Park Nomination Form.

<sup>94</sup> Clipping dated July 25, 1990, titled "Pillars are war memorial, landmark in east county," located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado. See also "Our Neighbors," *The Lafayette Leader*, Vol. XV, No. 15 (May 24, 1929), and "Local Items," *The Lafayette Leader*, Vol. XV, No. 17 (June 7, 1929).

<sup>95</sup> Legion Park Nomination Form.

<sup>96</sup> Undated photo from "A.A. Paddock Collection: East Boulder County," maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>97</sup> "Unable to Prevent Placing Billboards," *The Daily Times*, Vol. XXXIII, No. 235 (September 25, 1927).

<sup>98</sup> "Offers the Olive Branch," *Fort Collins Express-Courier*, (April 11, 1928).

<sup>99</sup> Undated photo from "A.A. Paddock Collection: East Boulder County," maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>100</sup> *Ibid.*

<sup>101</sup> Deed dated August 18, 1928 at Book 553, p. 464 in the records of the Boulder County Clerk and Recorder.

<sup>102</sup> Deeds dated May 19, 1930 recorded at Book 581, p. 380 and September 15, 1931 recorded at Book 582, p. 555 in the records of the Boulder County Clerk and Recorder. See also "New Flag for Nine Mile Corner," *The Lafayette Leader*, Vol. 38, No. 21 (April 17, 1943).

<sup>103</sup> "Boulder County Items," *The Lafayette Leader*, Vol. XVI, No. 31 (August 3, 1928).

<sup>104</sup> Undated photo from "A.A. Paddock Collection: East Boulder County," maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>105</sup> "Boulder County Items," *The Lafayette Leader*, Vol. XVI, No. 27 (July 6, 1928).

<sup>106</sup> "Federal Road Bureau Desires to Maintain State Highways," *Daily Times*, Vol. XXXIII, No. 254 (October 7, 1927).

<sup>107</sup> See Deed dated August 18, 1928 at Book 553, p. 464 in the records of the Boulder County Clerk and Recorder.

<sup>108</sup> Deed dated November 26, 1917 at Book 416, p. 278 in the records of the Boulder County Clerk and Recorder.

<sup>109</sup> Legion Park Nomination Form.

<sup>110</sup> Lease recorded March 20, 1933 at Book 612, p. 170 in the records of the Boulder County Clerk and Recorder.

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<sup>111</sup> *Ibid.*

<sup>112</sup> Legion Park Nomination Form.

<sup>113</sup> *Ibid.*

<sup>114</sup> In 1925, the United States Secretary of Agriculture approved a controversial plan to implement a system of marked and numbered “interstate highways” on major roads receiving federal funding. These numbered routes were intended to supersede and simplify the burgeoning numbers of privately sponsored “auto trails” such as the Lincoln Highway. See Weingroff, *supra*. Records indicate that the route from Lafayette to Longmont was initially included as part of US 85, later renumbered as US 285, then US 87, and finally the current US 287. At the same time, at least in the early years, the road was also referred to as Colorado State Highway 1. See Deed dated August 18, 1928 at Book 553, p. 464 in the records of the Boulder County Clerk and Recorder.

<sup>115</sup> Undated photo from “A.A. Paddock Collection: East Boulder County,” maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>116</sup> “New Flag for Nine Mile Corner,” *The Lafayette Leader*, Vol. 39, No. 21 (April 17, 1943).

<sup>117</sup> “Bond Flag to Fly at Nine Mile Corner,” *The Lafayette Leader*, Vol. 39, No. 23 (April 30, 1943).

<sup>118</sup> Undated photo from “A.A. Paddock Collection: East Boulder County,” maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>119</sup> Plat recorded June 24, 1963 at Plat Book 9, p. 39 in the records of the Boulder County Clerk and Recorder.

<sup>120</sup> Clipping dated July 25, 1990, titled “Pillars are war memorial, landmark in east county,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>121</sup> *The Lafayette Leader*, Vol. 71, No. 4 (February 24, 1972).

<sup>122</sup> *Ibid*; clipping dated March 13, 1972 from the *Longmont Times-Call* titled “Direct Hit Means Removal,” provided by the Longmont Museum; clipping dated July 25, 1990, titled “Pillars are war memorial, landmark in east county,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>123</sup> Legion Park Nomination Form.

<sup>124</sup> Inventory Record submitted with Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 12, 1980, obtained from the records of the Boulder County Planning Department.

<sup>125</sup> Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 20, 1980, obtained from the records of the Boulder County Planning Department.

<sup>126</sup> Inventory Record submitted with Architectural/Historical Component Form for Resource No. 5 BL 432 dated May 12, 1980, obtained from the records of the Boulder County Planning Department; information received from CDOT Region 4.

<sup>127</sup> USGS Aerial Photograph, Project GS ID taken July 14, 1949, from the Arthur Lakes Library at the Colorado School of Mines.

<sup>128</sup> Warranty Deed recorded February 1, 1983 at Reception No. 532304 in the records of the Boulder County Clerk and Recorder.

<sup>129</sup> Warranty Deeds recorded March 7, 1983 at Reception Nos. 536538 and 536539, and Order recorded July 30, 1984 at Reception No. 666149, in the records of the Boulder County Clerk and Recorder.

<sup>130</sup> No record has been located indicating that the south pillar was moved or modified. The 1980 Colorado Cultural Resource Survey form prepared by the Boulder County Historical Society (above) states that “mementos of the dedication” are kept inside the metal doors of the pillars. However, the reports from 1928 indicate that those mementos were sealed in the cornerstone of the south pillar. If the comment in the Survey is accurate, then it appears that the cornerstone was removed at some point. Whether, when and how that occurred is unknown.

<sup>131</sup> See <https://core.tdar.org/document/156704/project-fc-hes-007-2s-sh-7-and-us-287-cultural-resource-report-for-historical-resources> [accessed August 1, 2021].

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<sup>132</sup> Clipping dated April 2, 1983 titled “Monument topples,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>133</sup> “Monument at Highway 7-287 comes tumbling down,” *The Louisville Times*, Vol. 69, No. 44 (April 20, 1983).

<sup>134</sup> *Louisville Times*, Vol. 69, No. 44 (April 20, 1983).

<sup>135</sup> “Monument at Highway 7-287 comes tumbling down,” *The Louisville Times*, Vol. 69, No. 44 (April 20, 1983).

<sup>136</sup> “Monument at Highway 7-287 comes tumbling down,” *The Louisville Times*, Vol. 69, No. 44 (April 20, 1983); clipping dated April 2, 1983 titled “Monument topples,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>137</sup> “Monument to be rebuilt,” *The Louisville Times*, Vol. 69, No. 47 (May 11, 1983).

<sup>138</sup> Clipping dated July 9, 1983 titled “Monument going back up,” located in the Boulder Daily Camera Archives maintained by the Carnegie Library for Local History, Boulder, Colorado.

<sup>139</sup> Uncaptioned photo, *The Louisville Times*, Vol. 70, No. 3 (July 6, 1983).

<sup>140</sup> *The Louisville Times*, Vol. 70, No. 3 (July 6, 1983).

<sup>141</sup> Warranty Deed recorded March 7, 1983 at Reception No. 536538 in the records of the Boulder County Clerk and Recorder.

<sup>142</sup> Photo dated August 5, 1983 from the records of the Boulder County Planning Department.

<sup>143</sup> Munio, Natalie, “87-year-old-pillars devoted to WWI vets at US-287 & Arapahoe gaining new attention,” *Colorado Hometown Weekly* (November 10, 2015) found at <https://www.coloradohometownweekly.com/2015/11/10/87-year-old-pillars-devoted-to-wwi-vets-at-us-287-arapahoe-gaining-new-attention/> [accessed July 27, 2021].

<sup>144</sup> See <https://gallowayus.com/project/nine-mile-corner/> [accessed July 27, 2021].

<sup>145</sup> See Silo Subdivision Final Planned Unit Development (PUD), approved by the Lafayette City Council, October 17, 2017.

<sup>146</sup> Data obtained from Boulder County Assessor’s Office website found at <https://www.bouldercounty.org/departments/assessor/> [accessed September 7, 2021].

<sup>147</sup> “Arapahoe Road / CO State Highway 7: Planning & Environmental Linkage (PEL) Study Update,” presented to the Boulder County Open Space Board of Trustees, dated August 9, 2017.

[illegible]

PROPERTY BOUNDARY FINE

PROPOSED LOT LINE

ADJACENT PROPERTY BOUNDARY FINE

EASEMENT BOUNDARY FINE

FIRE ACCESS EASEMENT

SIDEWALK/PAVEMENT

LANDSCAPE BUFFER

EXISTING TO REMAIN

PROPOSED FILL

PROPOSED NEW

PROPOSED HEAVY DUTY ASPHALT PAVEMENT

PROPOSED 7.5% SLOPE, TIE, AND OVERLAY

PROPOSED CONCRETE PAVEMENT

PROPOSED ASPHALT PAVEMENT

[illegible][illegible]

- 3 EXISTING UNDERGROUND ELECTRIC TO BE REMAIN
- 4 EXISTING GAS LINE TO BE REMAIN
- 5 EXISTING IRRIGATION LINE TO BE REMAIN
- 6 EXISTING WATER MAIN TO BE REMAIN
- 7 EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- 8 EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- 9 PROPOSED STORM SEWER IN LINE (SEE STORM SEWER PLAN A PROFILE)
- 10 EXISTING STORM SEWER IN LINE (SEE STORM SEWER PLAN A PROFILE)
- 11 EXISTING TRAFFIC SIGNAL POLE AND MAST ROAD TO BE REMAIN
- 12 EXISTING STREET LIGHT TO BE REMAIN
- 13 EXISTING LIGHT TO BE REMOVED AND REPLACED WITH TYPE B INLET
- 14 PROPOSED FLOOD STREET LIGHT (SEE STORM SEWER PLAN A PROFILE)
- 15 EXISTING SANITARY MANHOLE TO BE REMAIN
- 16 PROPOSED SANITARY SEWER (SEE SANITARY SEWER PLAN A PROFILE)
- 17 PROPOSED WET LINE (SEE WET LINE PLAN A PROFILE)
- 18 EXISTING STORM SEWER TO BE REMAIN
- 19 EXISTING UNDERGROUND TELEPHONE LINE TO BE REMAIN
- 20 PROPOSED TRAFFIC SIGNAL POLE AND MAST (ALICE SIGNAL PLAN)
- 21 EXISTING SIGNAL SEWER TO BE REMAIN
- 22 PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT (SEE SIGNAL PLANS)
- 23 PROPOSED STREET LIGHT (GREEN SIGNAL LIGHT) (SEE SHEET 165.3)
- 24 EXISTING STORM SEWER MANHOLE TO BE REMAIN
- 25 PROPOSED BOX CULVERT (6.5' x 1' EXTENSION)
- 26 EXISTING BOX CULVERT TO REMAIN
- 27 EXISTING POWER POLE TO BE REMOVED
- 28 EXISTING POWER POLE TO BE RELOCATED
- 29 EXISTING POWER POLE AND WETTER TO BE RELOCATED
- 30 EXISTING OVERHEAD ELECTRIC LINE TO BE REMOVED AND RELOCATED
- 31 EXISTING UTILITY CASSINER BY SEPARATE DOCUMENT
- 32 EXISTING WET LINE TO BE REMOVED
- 33 PROPOSED STORM SEWER INLET (BY OTHERS)
- 34 PROPOSED STORM SEWER MANHOLE (BY OTHERS)
- 35 PROPOSED CHIMNEY (SEE REMOVAL DETAILS)
- 36 PROPOSED TWO-WAY CULVERT

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5' ALUMINUM CAP STAMPED "P1529752" IN A RANGE BOX TO THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5' ALUMINUM CAP STAMPED "P1512405" FOR THE NE CORNER AND IS ASSUMED TO BEAR S 69°50'1" E.

ELEVATIONS SHOWN ARE NAVD 80 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE IUGG SMARTNET RTK GPS NETWORK AND GEOID 128.

ALL UTILITY LOCATIONS SHOWN ARE BASED ON MAPS PROVIDED BY THE APPROPRIATE UTILITY COMPANY AND FIELD SURFACE EVIDENCE AT THE TIME OF SURVEY AND IS TO BE CONSIDERED AN

## 811

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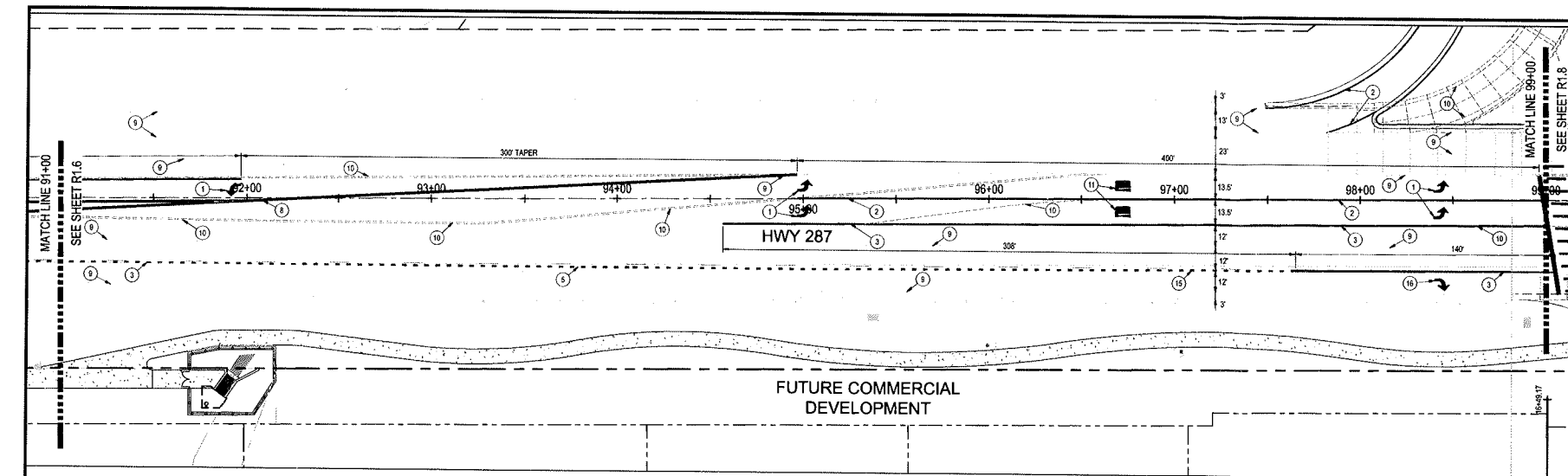


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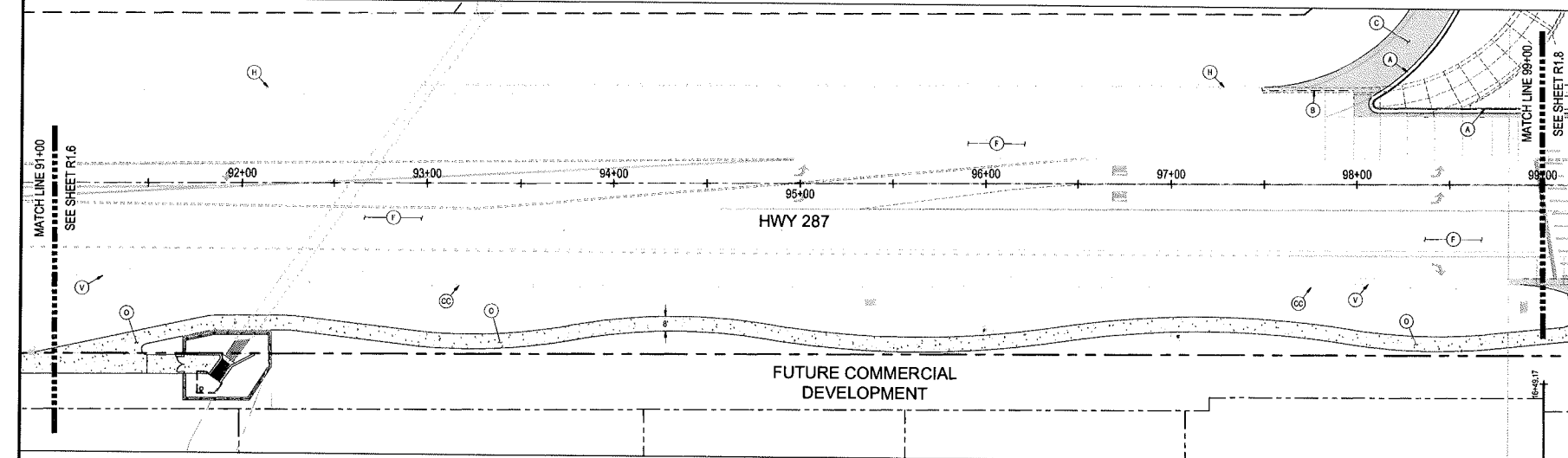
INFRASTRUCTURE CONSTRUCTION  
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NINE MILE CORNER  
EVERGREEN DEVCO INC

HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

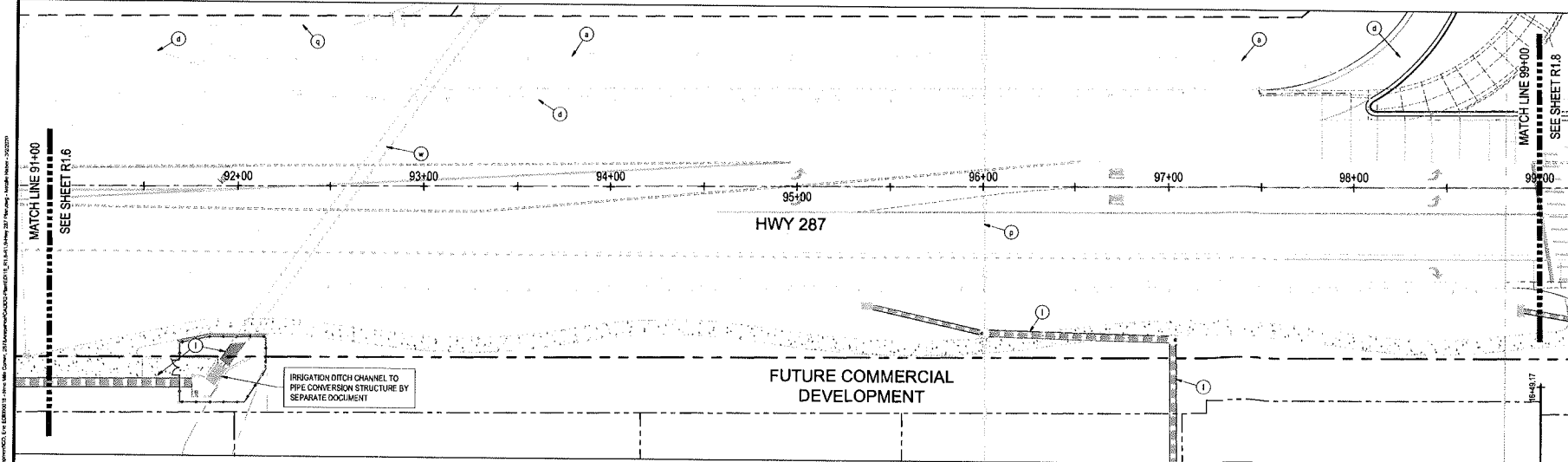




HIGHWAY 287 SIGNAGE & STRIPING PLAN: STA 91+00.00 TO STA 99+00.00  
SCALE: 1"=40'



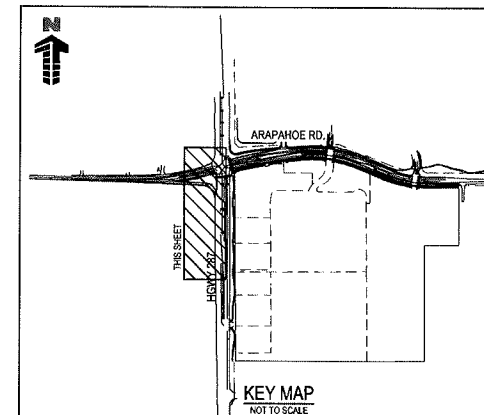
HIGHWAY 287 PAVING PLAN: STA 91+00.00 TO STA 99+00.00  
SCALE: 1"=40'



HIGHWAY 287 UTILITY PLAN: STA 91+00.00 TO STA 99+00.00  
SCALE: 1"=40'

### SIGNAGE & STRIPING SCHEDULE

- PROPOSED LEFT TURN SYMBOL, WHITE
- PROPOSED 4" WIDE SOLID LINE, WHITE
- PROPOSED 6" WIDE SOLID LINE, WHITE
- PROPOSED 4" WIDE SKIP LANE LINE, WHITE
- PROPOSED 4" WIDE DOTTED EXTENSION LINE, WHITE
- PROPOSED 24" WIDE STOP BAR, WHITE
- PROPOSED CROSSWALK STRIPING PER M.U.T.C.D., WHITE
- PROPOSED 4" DOUBLE SOLID YELLOW LINES
- EXISTING PAVEMENT MARKING TO REMAIN
- EXISTING PAVEMENT MARKING TO BE REMOVED
- PROPOSED "ONLY" TEXT, WHITE
- PROPOSED "STOP" SIGN
- PROPOSED "ONE WAY" SIGN
- PROPOSED 4" WIDE SOLID LINE, YELLOW
- PROPOSED 6" WIDE DOTTED EXTENSION LINE, WHITE
- PROPOSED RIGHT TURN SYMBOL, WHITE
- EXISTING "STOP" SIGN TO REMAIN
- PROPOSED "RIGHT LANE MUST TURN RIGHT" SIGN
- PROPOSED BUS STOP SIGN, BUS STOP LOCATION TO BE CONFIRMED WITH RTD



### PAVING PLAN SCHEDULE

- PROPOSED 6" CURB AND 2" GUTTER
- PROPOSED SAWCUT
- PROPOSED ASPHALT PAVEMENT
- PROPOSED ADA RAMP
- PROPOSED EDGE OF CONCRETE
- EXISTING ROADWAY PAVEMENT TO REMAIN
- EXISTING CURB AND GUTTER TO REMAIN
- EXISTING EDGE OF ASPHALT TO REMAIN
- EXISTING ADA RAMP TO REMAIN
- EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- EXISTING MONUMENT TO REMAIN
- EXISTING CURB AND GUTTER TO BE REMOVED
- PROPOSED SIDEWALK
- EXISTING INLET TO REMAIN
- EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- EXISTING STREET LIGHT TO REMAIN
- EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- EXISTING SIDEWALK TO REMAIN
- PROPOSED CONCRETE PAVEMENT
- EXISTING EDGE OF CONCRETE TO REMAIN
- PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM
- PROPOSED STAMPED CONCRETE MEDIAN TO MATCH EXISTING
- PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT
- PROPOSED 8-FT WIDE CONCRETE CROSSWALK
- PROPOSED RIP RAP
- PROPOSED ASPHALT MILL AND OVERLAY
- EXISTING GUARDRAIL TO REMAIN

### UTILITY PLAN SCHEDULE

- EXISTING UNDERGROUND ELECTRIC TO REMAIN
- EXISTING GAS LINE TO REMAIN
- EXISTING IRRIGATION LINE TO REMAIN
- EXISTING WATER MAIN TO REMAIN
- EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- PROPOSED STORM SEWER INLET (SEE STORM SEWER PLAN & PROFILES)
- EXISTING STORM SEWER INLET TO REMAIN
- EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- EXISTING STREET LIGHT TO REMAIN
- EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- PROPOSED STORM SEWER (SEE STORM SEWER PLAN & PROFILES)
- EXISTING SANITARY MANHOLE TO REMAIN
- PROPOSED SANITARY SEWER (SEE SANITARY SEWER PLAN & PROFILES)
- PROPOSED WATER LINE (SEE WATER PLAN)
- EXISTING STORM SEWER TO REMAIN
- EXISTING UNDERGROUND TELEPHONE LINE TO REMAIN
- PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM
- EXISTING 24-INCH STORM SEWER TO REMAIN
- PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT
- PROPOSED STREET LIGHT, 250-WATT HIGH PRESSURE SODIUM, 35-FT IN HEIGHT
- EXISTING STORM SEWER MANHOLE TO REMAIN
- EXISTING BOX CULVERT UNDER HIGHWAY 287
- EXISTING BOX CULVERT TO REMAIN
- EXISTING POWER POLE TO REMAIN
- EXISTING POWER POLE TO BE RELOCATED
- EXISTING POWER POLE TO BE RELOCATED
- EXISTING WATER METER TO REMAIN
- EXISTING POWER POLE AND METER TO BE RELOCATED
- EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- 48" UTILITY EASEMENT
- EXISTING WATER LINE TO BE REMOVED
- PROPOSED STORM SEWER INLET (BY OTHERS)
- PROPOSED STORM SEWER MANHOLE (BY OTHERS)

### ROADWAY LEGEND

---	PROPERTY BOUNDARY LINE
---	PROPOSED LOT LINE
---	ADJACENT PROPERTY BOUNDARY LINE
---	EASEMENT BOUNDARY LINE
---	FIRE ACCESS EASEMENT
---	SIDEWALK EASEMENT
---	LANDSCAPE BUFFER
---	EXISTING TO REMAIN
---	PROPOSED FUTURE
---	PROPOSED NEW
---	PROPOSED HEAVY DUTY ASPHALT PAVEMENT
---	PROPOSED 2" ASPHALT MILL AND OVERLAY
---	PROPOSED CONCRETE PAVEMENT

### UTILITY LEGEND

---	EXISTING MINOR CONTOUR
---	EXISTING MAJOR CONTOUR
---	PROPOSED MINOR CONTOUR
---	PROPOSED MAJOR CONTOUR
---	PROPERTY BOUNDARY LINE
---	PROPOSED LOT LINE
---	ADJACENT PROPERTY BOUNDARY LINE
---	EASEMENT BOUNDARY LINE
---	EXISTING WATER LINE
---	EXISTING SANITARY SEWER
---	EXISTING STORM SEWER
---	EXISTING UNDERGROUND GAS LINE
---	EXISTING UNDERGROUND ELECTRICAL
---	EXISTING UNDERGROUND TELEPHONE
---	EXISTING FIBER OPTIC LINE
---	STORM SEWER BY OTHERS (LESS THAN 12")
---	STORM SEWER BY OTHERS (GREATER THAN 12")
---	SANITARY SEWER BY OTHERS
---	WATER LINE BY OTHERS
---	PROPOSED WATER LINE
---	PROPOSED SANITARY SEWER
---	PROPOSED STORM SEWER (GREATER THAN 12")
---	PROPOSED UNDERGROUND GAS LINE
---	PROPOSED UNDERGROUND ELECTRICAL
---	PROPOSED UNDERGROUND TELEPHONE
---	PROPOSED GUARDRAIL
---	EXISTING WATER VALVE
---	EXISTING MANHOLE
---	PROPOSED WATER METER
---	PROPOSED WATER VALVE
---	PROPOSED FIRE HYDRANT
---	PROPOSED MANHOLE
---	UTILITY PEDESTAL
---	PROPOSED ELECTRICAL TRANSFORMER
---	PROPOSED INLET

### BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5" ALUMINUM CAP STAMPED "PLS29192" IN A RANGE BOX FOR THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5" ALUMINUM CAP STAMPED "PLS1209" FOR THE NE CORNER AND IS ASSUMED TO BEAR S 89°58'11"E.

### BENCHMARK

ELEVATIONS SHOWN ARE NAVD 83 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE LIECA SMARTNET RTK GPS NETWORK AND GEOID 12B.

### CAUTION - NOTICE TO CONTRACTOR

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- WHERE A PROPOSED UTILITY CROSSES AN EXISTING UTILITY, IT IS THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF SUCH EXISTING UTILITY, EITHER THROUGH POT-HOLING OR ALTERNATIVE METHOD. REPORT INFORMATION TO THE ENGINEER PRIOR TO CONSTRUCTION.



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**INFRASTRUCTURE CONSTRUCTION DOCUMENTS**  
**NINE MILE CORNER**  
**EVERGREEN DEVCO INC**  
**HIGHWAY 287 & ARAPAHOE RD.**  
**ERIE, COLORADO**

#	Date	Issue / Description	Init.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/28/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/17/20	FINAL STAMPED INF. SET	BSM

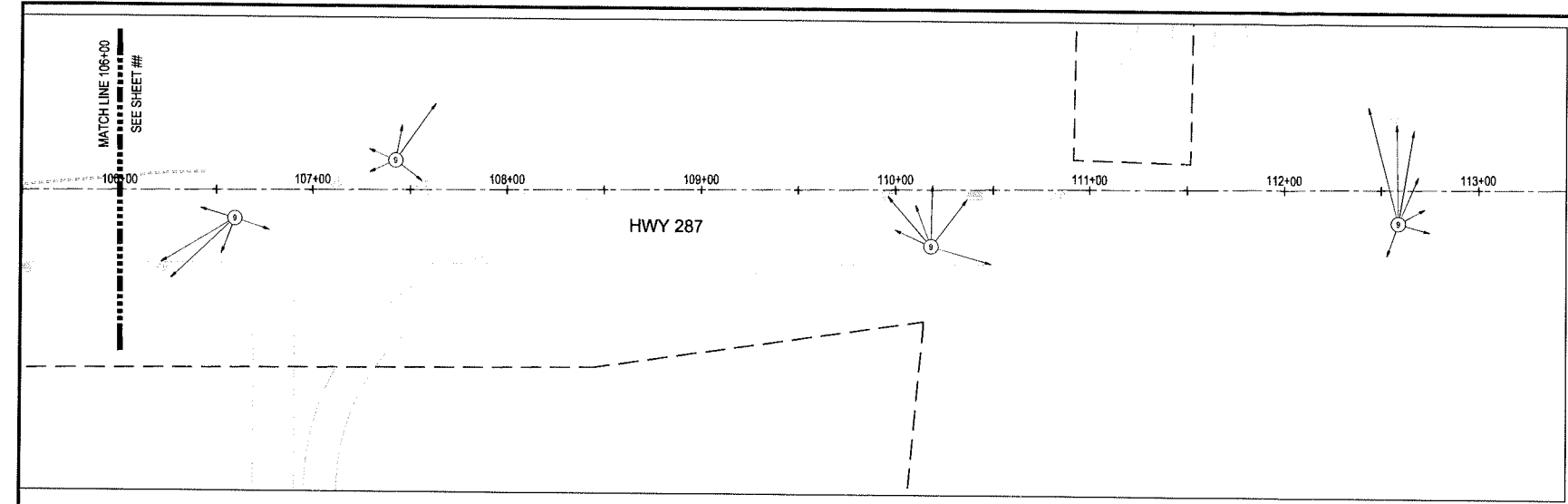
Project No:	ED1000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

HIGHWAY 287 SITE PLAN

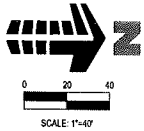
**R1.7**



INFRASTRUCTURE CONSTRUCTION  
DOCUMENTS  
NINE MILE CORNER  
EVERGREEN DEVCO INC  
  
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

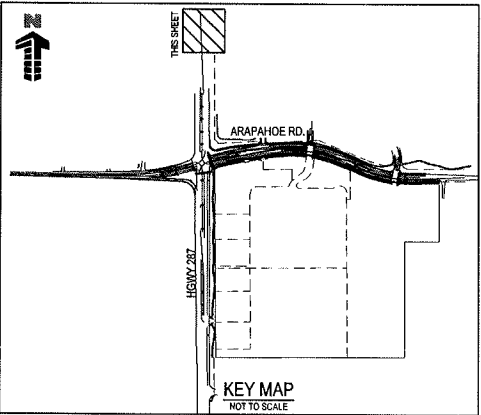


HIGHWAY 287 SIGNAGE & STRIPING PLAN: STA 106+00.00 TO STA 113+00.00  
SCALE: 1"=40'



SIGNAGE & STRIPING SCHEDULE

- 1 PROPOSED LEFT TURN SYMBOL, WHITE
- 2 PROPOSED 4" WIDE SOLID LINE, WHITE
- 3 PROPOSED 8" WIDE SOLID LINE, WHITE
- 4 PROPOSED 4" WIDE SKIP LANE LINE, WHITE
- 5 PROPOSED 4" WIDE DOTTED EXTENSION LINE, WHITE
- 6 PROPOSED 24" WIDE STOP BAR, WHITE
- 7 PROPOSED CROSSWALK STRIPING PER MUT.C.D., WHITE
- 8 PROPOSED 4" DOUBLE SOLID YELLOW LINES
- 9 EXISTING PAVEMENT MARKING TO REMAIN
- 10 EXISTING PAVEMENT MARKING TO BE REMOVED
- 11 PROPOSED "ONLY" TEXT, WHITE
- 12 PROPOSED "STOP" SIGN
- 13 PROPOSED "ONE WAY" SIGN
- 14 PROPOSED 4" WIDE SOLID LINE, YELLOW
- 15 PROPOSED 8" WIDE DOTTED EXTENSION LINE, WHITE
- 16 PROPOSED RIGHT TURN SYMBOL, WHITE
- 17 EXISTING "STOP" SIGN TO REMAIN
- 18 PROPOSED "RIGHT LANE MUST TURN RIGHT" SIGN
- 19 PROPOSED BUS STOP SIGN. BUS STOP LOCATION TO BE CONFIRMED WITH RTD



ROADWAY LEGEND

	PROPERTY BOUNDARY LINE
	PROPOSED LOT LINE
	ADJACENT PROPERTY BOUNDARY LINE
	EASEMENT BOUNDARY LINE
	FIRE ACCESS EASEMENT
	SIDEWALK EASEMENT
	LANDSCAPE BUFFER
	EXISTING TO REMAIN
	PROPOSED FUTURE
	PROPOSED NEW
	PROPOSED HEAVY DUTY ASPHALT PAVEMENT
	PROPOSED 2" ASPHALT MILL AND OVERLAY
	PROPOSED CONCRETE PAVEMENT

UTILITY LEGEND

	EXISTING MINOR CONTOUR
	EXISTING MAJOR CONTOUR
	PROPOSED MINOR CONTOUR
	PROPOSED MAJOR CONTOUR
	PROPERTY BOUNDARY LINE
	PROPOSED LOT LINE
	ADJACENT PROPERTY BOUNDARY LINE
	EASEMENT BOUNDARY LINE
	EXISTING WATER LINE
	EXISTING SANITARY SEWER
	EXISTING STORM SEWER
	EXISTING UNDERGROUND GAS LINE
	EXISTING UNDERGROUND ELECTRICAL
	EXISTING FIBER OPTIC LINE
	STORM SEWER BY OTHERS (LESS THAN 12')
	STORM SEWER BY OTHERS (GREATER THAN 12')
	SANITARY SEWER BY OTHERS
	WATER LINE BY OTHERS
	PROPOSED WATER LINE
	PROPOSED SANITARY SEWER
	PROPOSED STORM SEWER (GREATER THAN 12')
	PROPOSED UNDERGROUND GAS LINE
	PROPOSED UNDERGROUND ELECTRICAL
	PROPOSED UNDERGROUND TELEPHONE
	PROPOSED GUARDRAIL
	EXISTING WATER VALVE
	EXISTING MANHOLE
	PROPOSED WATER METER
	PROPOSED WATER VALVE
	PROPOSED FIRE HYDRANT
	PROPOSED MANHOLE
	UTILITY PEDESTAL
	PROPOSED ELECTRICAL TRANSFORMER
	PROPOSED INLET

PAVING PLAN SCHEDULE

- A PROPOSED 6" CURB AND 2" GUTTER
- B PROPOSED SAWCUT
- C PROPOSED ASPHALT PAVEMENT
- D PROPOSED ADA RAMP
- E PROPOSED EDGE OF CONCRETE
- F EXISTING ROADWAY PAVEMENT TO REMAIN
- G EXISTING CURB AND GUTTER TO REMAIN
- H EXISTING EDGE OF ASPHALT TO REMAIN
- J EXISTING ADA RAMP TO REMAIN
- K EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- L EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- M EXISTING MONUMENT TO REMAIN
- N EXISTING CURB AND GUTTER TO BE REMOVED
- O PROPOSED SIDEWALK
- P EXISTING INLET TO REMAIN
- Q EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- R EXISTING STREET LIGHT TO REMAIN
- S EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- T EXISTING SIDEWALK TO REMAIN
- U PROPOSED CONCRETE PAVEMENT
- V EXISTING EDGE OF CONCRETE TO REMAIN
- W PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM
- X PROPOSED STAMPED CONCRETE MEDIAN TO MATCH EXISTING
- Y PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT
- Z PROPOSED 8-FT WIDE CONCRETE CROSSSPAN
- AA PROPOSED RIP RAP
- BB PROPOSED ASPHALT MILL AND OVERLAY
- CC EXISTING GUARDRAIL TO REMAIN

UTILITY PLAN SCHEDULE

- A EXISTING UNDERGROUND ELECTRIC TO REMAIN
- B EXISTING GAS LINE TO REMAIN
- C EXISTING IRRIGATION LINE TO REMAIN
- D EXISTING WATER MAIN TO REMAIN
- E EXISTING TRAFFIC SIGNAL TO BE REMOVED AND RELOCATED
- F EXISTING TRAFFIC SIGNAL EQUIPMENT TO BE REMOVED AND RELOCATED
- G PROPOSED STORM SEWER INLET (SEE STORM SEWER PLAN & PROFILES)
- H EXISTING STORM SEWER INLET TO REMAIN
- I EXISTING TRAFFIC SIGNAL POLE AND MAST ARM TO REMAIN
- J EXISTING STREET LIGHT TO REMAIN
- K EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- L PROPOSED STORM SEWER (SEE STORM SEWER PLAN & PROFILES)
- M EXISTING SANITARY MANHOLE TO REMAIN
- N PROPOSED SANITARY SEWER (SEE SANITARY SEWER PLAN & PROFILES)
- O PROPOSED WATER LINE (SEE WATER PLAN)
- P EXISTING STORM SEWER TO REMAIN
- Q EXISTING UNDERGROUND TELEPHONE LINE TO REMAIN
- R PROPOSED TRAFFIC SIGNAL POLE AND MAST ARM
- S EXISTING 24-INCH STORM SEWER TO REMAIN
- T PROPOSED LOCATION OF RELOCATED TRAFFIC SIGNAL EQUIPMENT
- U PROPOSED STREET LIGHT, 250-WATT HIGH PRESSURE SODIUM, 35-FT IN HEIGHT
- V EXISTING STORM SEWER MANHOLE TO REMAIN
- W EXISTING BOX CULVERT UNDER HIGHWAY 287
- X EXISTING BOX CULVERT TO REMAIN
- Y EXISTING POWER POLE TO REMAIN
- Z EXISTING POWER POLE TO BE RELOCATED
- AA EXISTING WATER METER TO REMAIN
- BB EXISTING POWER POLE AND METER TO BE RELOCATED
- CC EXISTING INLET TO BE REMOVED AND REPLACED WITH TYPE R INLET
- DD 42" UTILITY EASEMENT
- EE EXISTING WATER LINE TO BE REMOVED
- FF PROPOSED STORM SEWER INLET (BY OTHERS)
- GG PROPOSED STORM SEWER MANHOLE (BY OTHERS)

BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2.5" ALUMINUM CAP STAMPED "PLS12012" IN A RANGE BOX FOR THE N 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2.5" ALUMINUM CAP STAMPED "PLS12405" FOR THE NE CORNER AND IS ASSUMED TO BEAR S 89°50'01" E.

BENCHMARK

ELEVATIONS SHOWN ARE NAVD 83 AND ARE DERIVED VIA GPS OBSERVATIONS FROM THE LIECA SMARTNET RTK GPS NETWORK AND GEOID 12B.

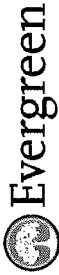
CAUTION - NOTICE TO CONTRACTOR

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INFRASTRUCTURE CONSTRUCTION DOCUMENTS  
NINE MILE CORNER  
EVERGREEN DEVCO INC  
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

#	Date	Issue / Description	Init.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/20/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/17/20	FINAL STAMPED INF. SET.	BSM

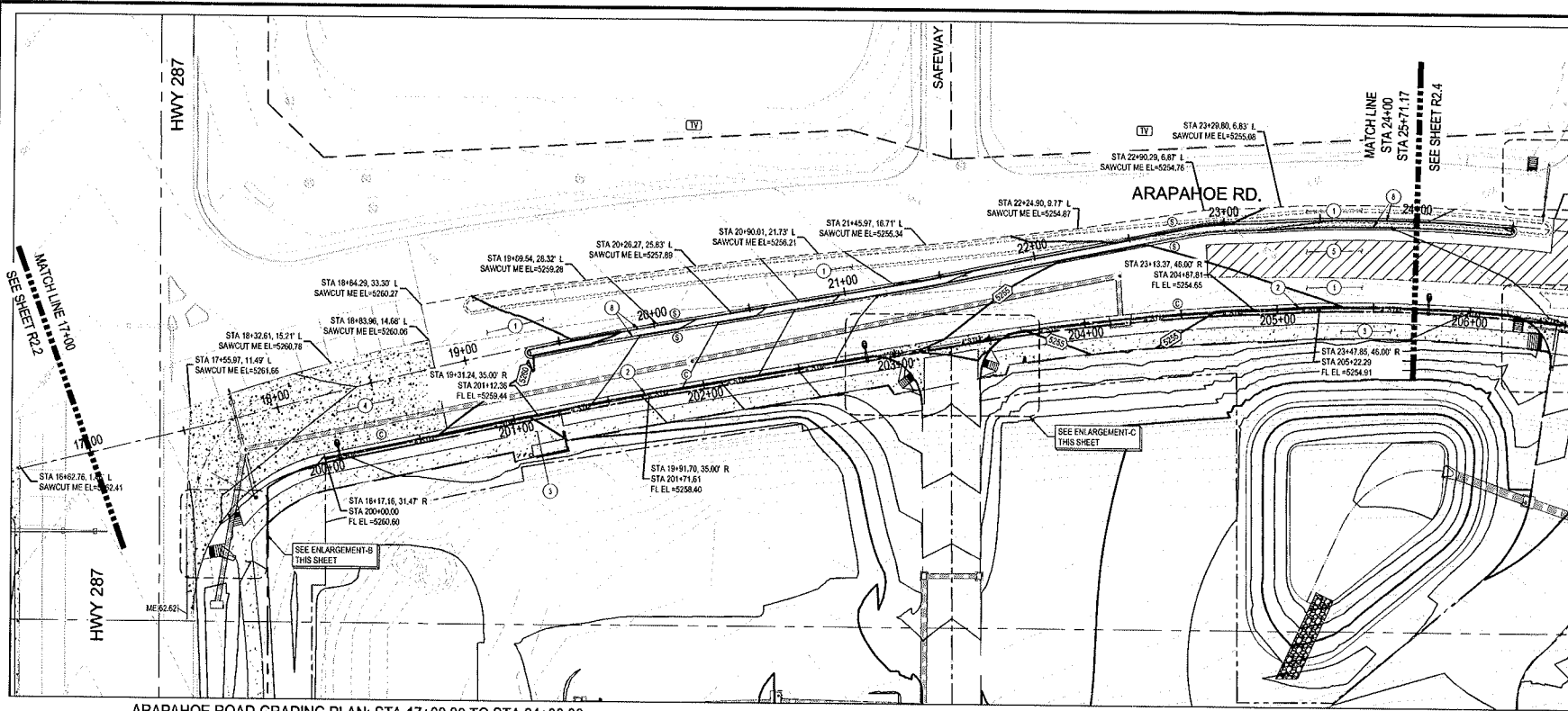
Project No:	ED000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

HIGHWAY 287 SITE PLAN

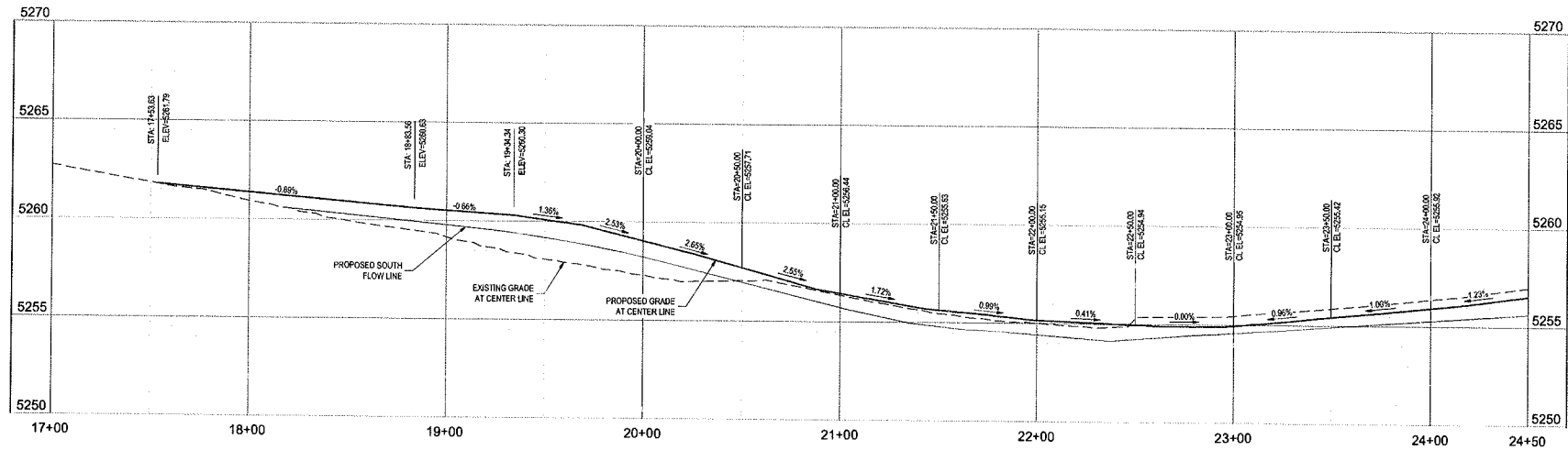
R1.9



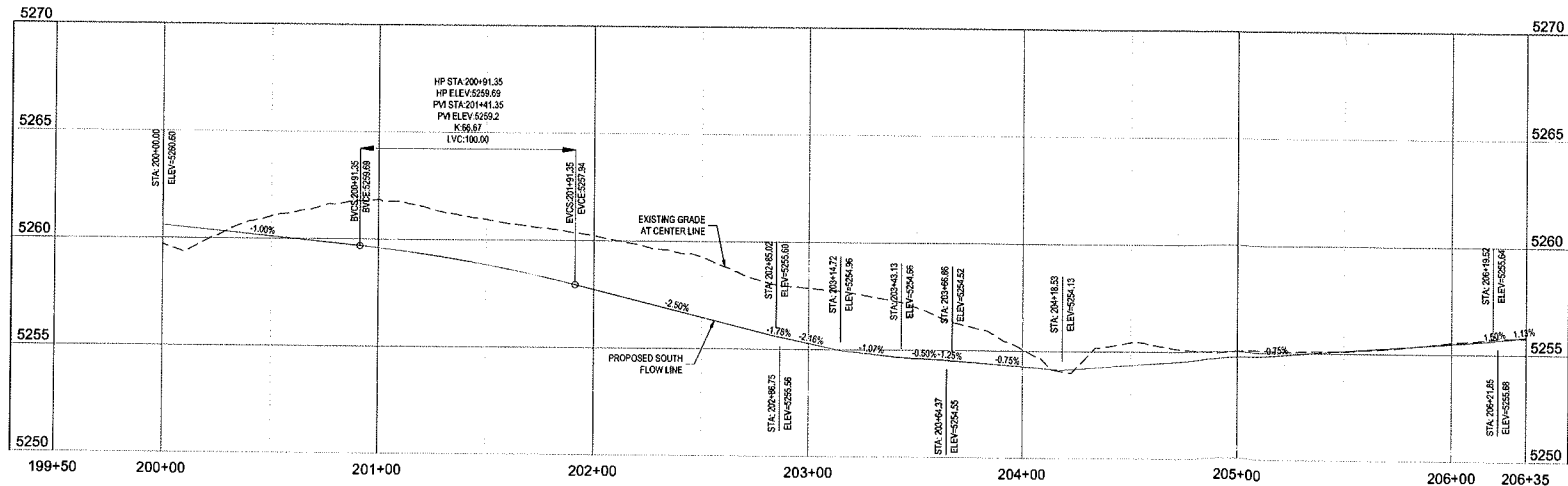




ARAPAHOE ROAD GRADING PLAN: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40'



ARAPAHOE ROAD GRADING PROFILE: STA 17+00.00 TO STA 24+00.00  
SCALE: 1"=40' HORIZ, 1"=4' VERT



ARAPAHOE ROAD SOUTH FLOW LINE PROFILE: STA 20+00.00 TO STA 25+71.17  
SCALE: 1"=40' HORIZ, 1"=4' VERT

#### GRADING SCHEDULE

- PROPOSED HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED 6" CURB AND 2" GUTTER
- PROPOSED SIDEWALK
- PROPOSED CONCRETE PAVEMENT
- PROPOSED ASPHALT MILL AND OVERLAY
- PROPOSED ADA RAMP
- PROPOSED GUARDRAIL
- PROPOSED 6" CURB AND 1" GUTTER

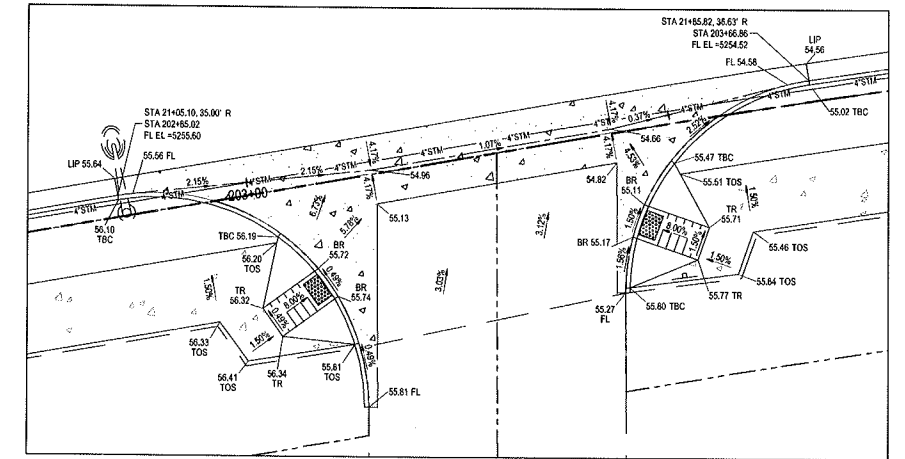
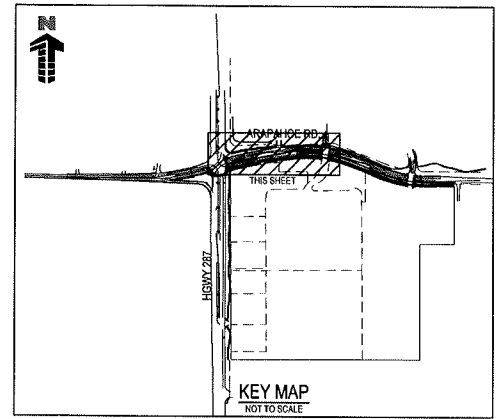


#### GRADING LEGEND

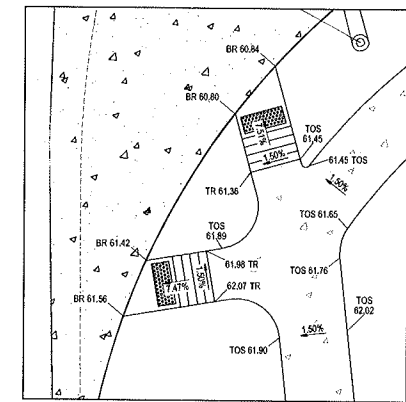
- EXISTING MINOR CONTOUR
- EXISTING MAJOR CONTOUR
- PROPOSED MINOR CONTOUR
- PROPOSED MAJOR CONTOUR
- CATCH CURB AND GUTTER
- SPILL CURB AND GUTTER

#### ROADWAY LEGEND

- PROPERTY BOUNDARY LINE
- PROPOSED LOT LINE
- ADJACENT PROPERTY BOUNDARY LINE
- EASEMENT BOUNDARY LINE
- FIRE ACCESS EASEMENT
- SIDEWALK EASEMENT
- LANDSCAPE BUFFER
- EXISTING TO REMAIN
- PROPOSED FUTURE
- PROPOSED NEW
- PROPOSED HEAVY DUTY ASPHALT PAVEMENT
- PROPOSED 2" ASPHALT MILL AND OVERLAY
- PROPOSED CONCRETE PAVEMENT



ENLARGEMENT-C  
SCALE: 1"=10'



ENLARGEMENT-B  
SCALE: 1"=10'

#### BASIS OF BEARING

BEARINGS ARE BASED ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 34 AND MONUMENTED ON THE WEST BY A 2" ALUMINUM CAP STAMPED 'PLS2872' IN A RANGE BOX FOR THE 1/4 CORNER AND MONUMENTED ON THE EAST BY A 2" ALUMINUM CAP STAMPED 'PLS12405' FOR THE NE CORNER AND IS ASSUMED TO BEAR S 89°50'1" E.

#### BENCHMARK

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**Galloway**

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**Evergreen**  
Development | Services | Investments



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INFRASTRUCTURE CONSTRUCTION DOCUMENTS

NINE MILE CORNER  
EVERGREEN DEVCO INC

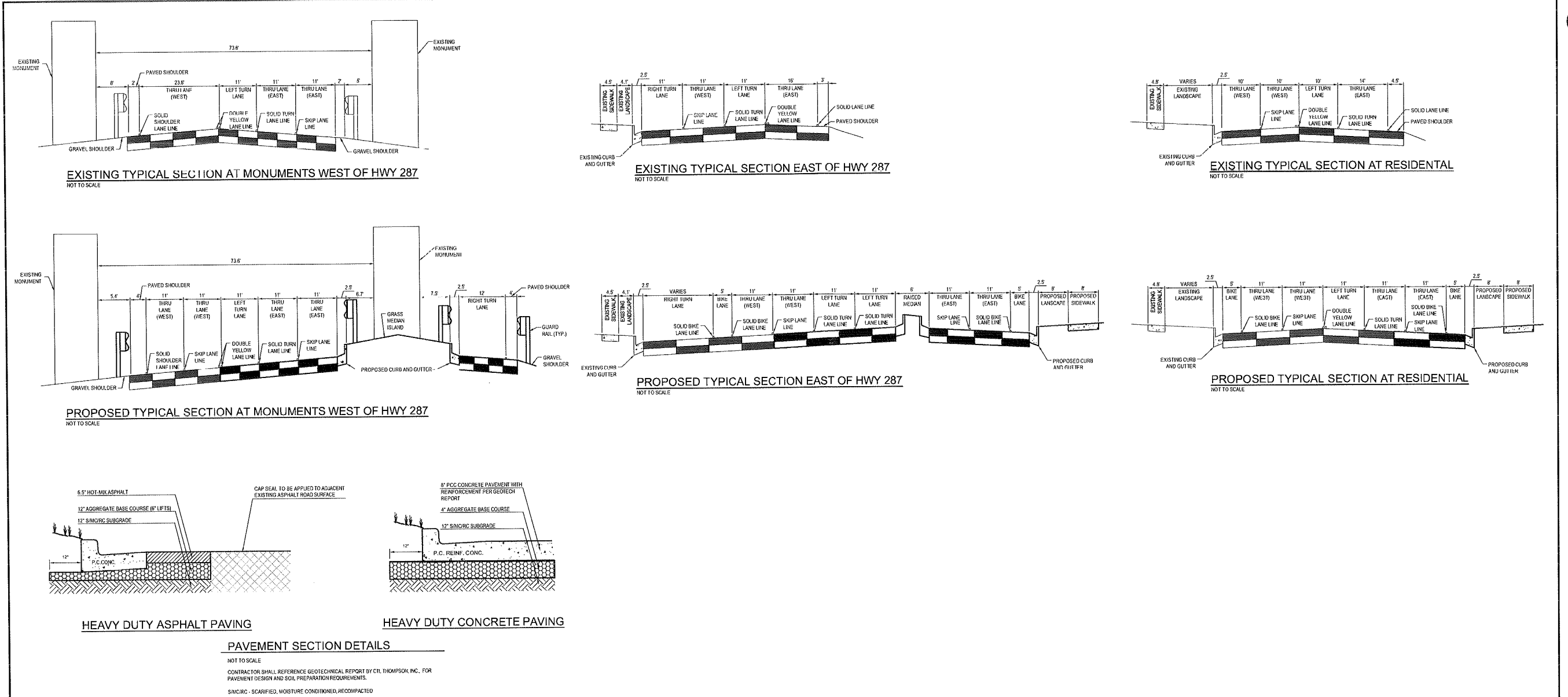
HIGHWAY 287 & ARAPAHOE RD.  
ERIE, COLORADO

#	Date	Issue / Description	Init.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/20/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/17/20	FINAL STAMPED INF. SET	BSM

Project No:	ED000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

ARAPAHOE ROAD GRADING PLAN & PROFILE

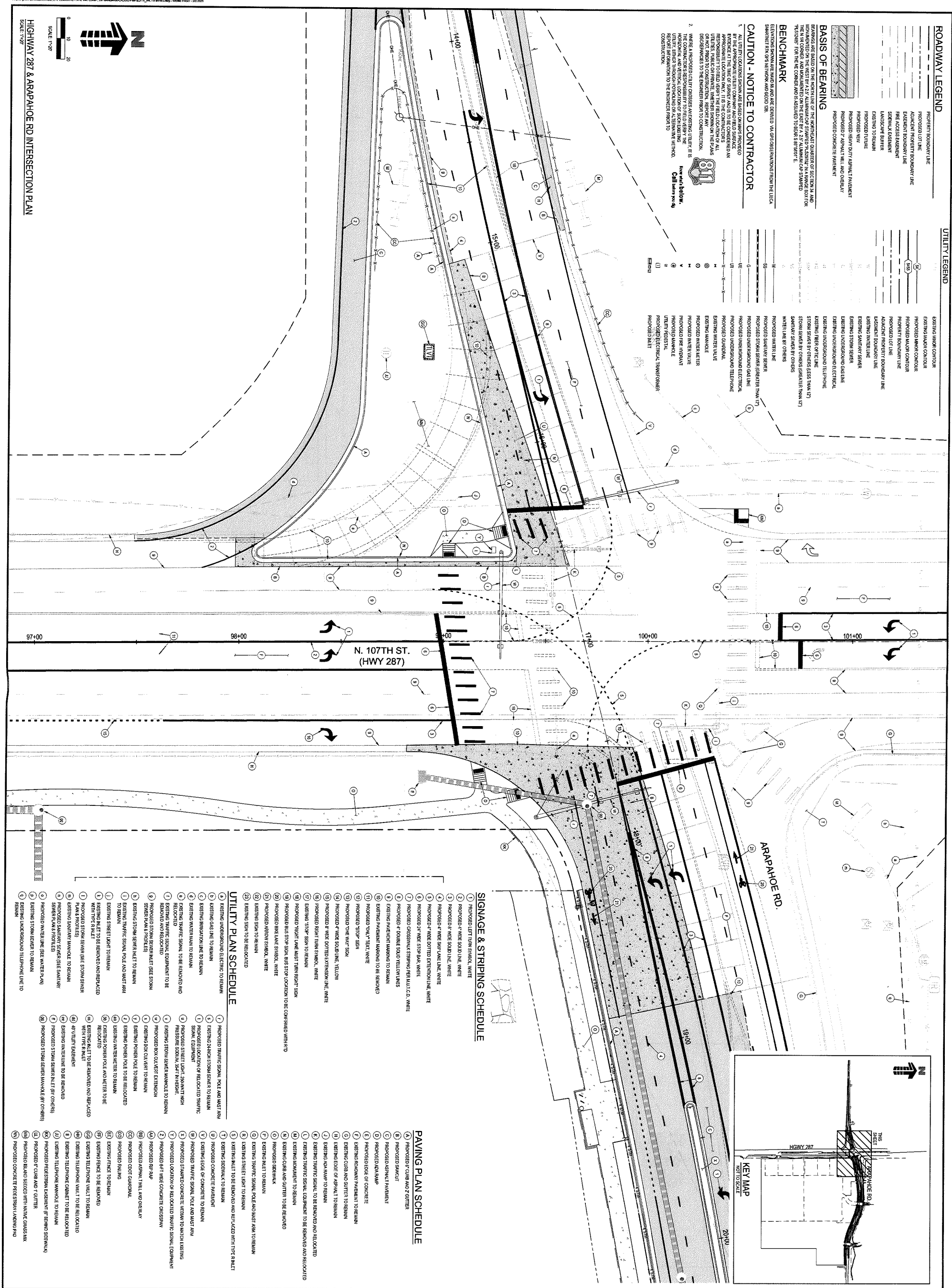
**R2.3**



#	Date	Issue / Description	Int.
1	11/15/19	4TH INFRASTRUC. SUB.	ACJ
2	01/24/20	5TH INFRASTRUC. SUB.	ACJ
3	02/20/20	6TH INFRASTRUC. SUB.	ACJ
4	03/03/20	7TH INFRASTRUC. SUB.	ACJ
5	03/05/20	BID SET ADM. 1	ACJ
6	03/17/20	BID SET ADM. 2	ACJ

Project No:	ED0000018
Drawn By:	RDG
Checked By:	BSM
Date:	9/3/19

ARAPAHOE ROAD TYPICAL  
SECTIONS





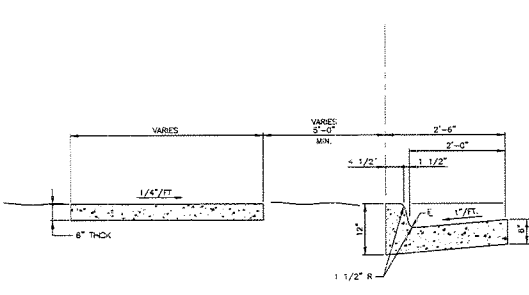
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**INFRASTRUCTURE CONSTRUCTION DOCUMENTS**  
**NINE MILE CORNER**  
**EVERGREEN DEVCO INC**  
  
**HIGHWAY 287 & ARAPAHOE RD.**  
**ERIE, COLORADO**

#	Date	Issue / Description	Int.
1	11/19/19	4TH INFRASTRUCTURE SUB. ACI	
2	01/24/20	2ND INFRASTRUCTURE SUB. ACI	
3	02/20/20	3RD INFRASTRUCTURE SUB. ACI	
4	03/03/20	4TH INFRASTRUCTURE SUB. ACI	
5	03/17/20	FINAL STAMPED P&E SET. BSM	

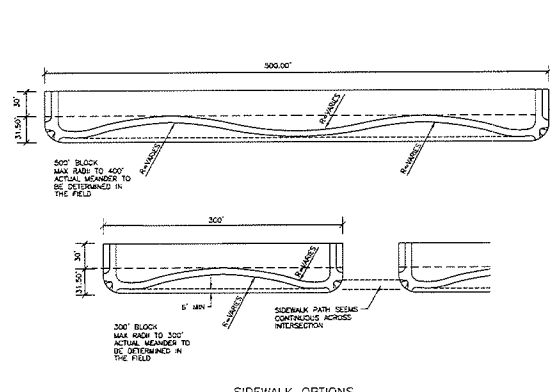
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Drawn By:	BSM
Checked By:	BSM
Date:	8/9/19

**R5.2**



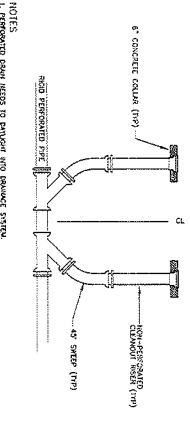
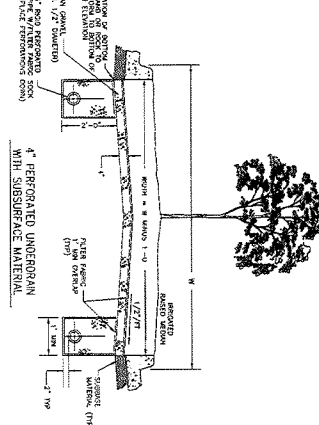
- NOTES**
1. SIDEWALK WIDTH SHALL BE SPECIFIED ON THE CONSTRUCTION PLANS.
  2. LANDSCAPE TREATMENT MUST BE SPECIFIED.
  3. 6" VERTICAL CURB, GUTTER, AND SIDEWALK IS REQUIRED FOR COLLECTOR AND ARTERIAL STREETS.

**The Town of ERIE COLORADO**  
DESIGNED BY: **6" VERTICAL CURB, GUTTER AND DETACHED SIDEWALK**  
DRAWN BY: **D. JENSEN**  
APPROVED BY: **D. JENSEN**  
DATE: **01/20/14**



**SIDEWALK OPTIONS**

**The Town of ERIE COLORADO**  
DESIGNED BY: **SIDEWALK DESIGN STANDARDS**  
DRAWN BY: **SW14**  
APPROVED BY: **D. JENSEN**  
DATE: **01/20/09**



**The Town of ERIE COLORADO**  
DESIGNED BY: **4" PERFORATED MEDIAN UNDERDRAIN FOR CENTER PLANNING**  
DRAWN BY: **SW17A**  
APPROVED BY: **D. JENSEN**  
DATE: **01/20/15**

<b>Computer File Information</b> PROJECT NO. ED000018 SHEET NO. M-608-1 SHEET TITLE: <b>6" VERTICAL CURB, GUTTER AND DETACHED SIDEWALK</b> DRAWN BY: <b>D. JENSEN</b> DATE: <b>01/20/14</b>		<b>Sheet Revision</b> REVISION NO. 1 REVISION DATE: 01/20/14 REVISION DESCRIPTION: <b>6" VERTICAL CURB, GUTTER AND DETACHED SIDEWALK</b>	
<b>Colorado Department of Transportation</b> Division of Project Support JMW/TA		<b>STANDARD PLAN NO.</b> M-608-1 Sheet No. 10 of 10	

**DETECTABLE WARNING SURFACE DETAILS**

**CURB RAMPS**

**SECTION A-A: CURB RAMP**

**SECTION B-B: CURB RAMP**

**SECTION C-C: CURB RAMP**

**SECTION D-D: CURB RAMP**

**SECTION E-E: CURB RAMP**

**SECTION F-F: CURB RAMP**

**SECTION G-G: CURB RAMP**

**SECTION H-H: CURB RAMP**

**SECTION I-I: CURB RAMP**

**SECTION J-J: CURB RAMP**

**SECTION K-K: CURB RAMP**

**SECTION L-L: CURB RAMP**

**SECTION M-M: CURB RAMP**

**SECTION N-N: CURB RAMP**

**SECTION O-O: CURB RAMP**

**SECTION P-P: CURB RAMP**

**SECTION Q-Q: CURB RAMP**

**SECTION R-R: CURB RAMP**

**SECTION S-S: CURB RAMP**

**SECTION T-T: CURB RAMP**

**SECTION U-U: CURB RAMP**

**SECTION V-V: CURB RAMP**

**SECTION W-W: CURB RAMP**

**SECTION X-X: CURB RAMP**

**SECTION Y-Y: CURB RAMP**

**SECTION Z-Z: CURB RAMP**

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<b>Colorado Department of Transportation</b> Division of Project Support JMW/TA		<b>STANDARD PLAN NO.</b> M-608-1 Sheet No. 10 of 10	

**DETECTABLE WARNING SURFACE DETAILS**

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**SECTION V-V: CURB RAMP**

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<b>Colorado Department of Transportation</b> Division of Project Support JMW/TA		<b>STANDARD PLAN NO.</b> M-608-1 Sheet No. 10 of 10	

**DETECTABLE WARNING SURFACE DETAILS**

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**SECTION N-N: CURB RAMP**

**SECTION O-O: CURB RAMP**

**SECTION P-P: CURB RAMP**

**SECTION Q-Q: CURB RAMP**

**SECTION R-R: CURB RAMP**

**SECTION S-S: CURB RAMP**

**SECTION T-T: CURB RAMP**

**SECTION U-U: CURB RAMP**

**SECTION V-V: CURB RAMP**

**SECTION W-W: CURB RAMP**

**SECTION X-X: CURB RAMP**

**SECTION Y-Y: CURB RAMP**

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<b>Colorado Department of Transportation</b> Division of Project Support JMW/TA		<b>STANDARD PLAN NO.</b> M-608-1 Sheet No. 2 of 10	

**PERPENDICULAR RAMP**

**SECTION A-A: PERPENDICULAR RAMP**

**SECTION B-B: PERPENDICULAR RAMP**

**SECTION C-C: PERPENDICULAR RAMP**

**SECTION D-D: PERPENDICULAR RAMP**

**SECTION E-E: PERPENDICULAR RAMP**

**SECTION F-F: PERPENDICULAR RAMP**

**SECTION G-G: PERPENDICULAR RAMP**

**SECTION H-H: PERPENDICULAR RAMP**

**SECTION I-I: PERPENDICULAR RAMP**

**SECTION J-J: PERPENDICULAR RAMP**

**SECTION K-K: PERPENDICULAR RAMP**

**SECTION L-L: PERPENDICULAR RAMP**

**SECTION M-M: PERPENDICULAR RAMP**

**SECTION N-N: PERPENDICULAR RAMP**

**SECTION O-O: PERPENDICULAR RAMP**

**SECTION P-P: PERPENDICULAR RAMP**

**SECTION Q-Q: PERPENDICULAR RAMP**

**SECTION R-R: PERPENDICULAR RAMP**

**SECTION S-S: PERPENDICULAR RAMP**

**SECTION T-T: PERPENDICULAR RAMP**

**SECTION U-U: PERPENDICULAR RAMP**

**SECTION V-V: PERPENDICULAR RAMP**

**SECTION W-W: PERPENDICULAR RAMP**

**SECTION X-X: PERPENDICULAR RAMP**

**SECTION Y-Y: PERPENDICULAR RAMP**

**SECTION Z-Z: PERPENDICULAR RAMP**

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<b>Colorado Department of Transportation</b> Division of Project Support JMW/TA		<b>STANDARD PLAN NO.</b> M-608-1 Sheet No. 1 of 4	

**PERPENDICULAR RAMP**

**SECTION A-A: PERPENDICULAR RAMP**

**SECTION B-B: PERPENDICULAR RAMP**

**SECTION C-C: PERPENDICULAR RAMP**

**SECTION D-D: PERPENDICULAR RAMP**

**SECTION E-E: PERPENDICULAR RAMP**

**SECTION F-F: PERPENDICULAR RAMP**

**SECTION G-G: PERPENDICULAR RAMP**

**SECTION H-H: PERPENDICULAR RAMP**

**SECTION I-I: PERPENDICULAR RAMP**

**SECTION J-J: PERPENDICULAR RAMP**

**SECTION K-K: PERPENDICULAR RAMP**

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**SECTION Q-Q: PERPENDICULAR RAMP**

**SECTION R-R: PERPENDICULAR RAMP**

**SECTION S-S: PERPENDICULAR RAMP**

**SECTION T-T: PERPENDICULAR RAMP**

**SECTION U-U: PERPENDICULAR RAMP**

**SECTION V-V: PERPENDICULAR RAMP**

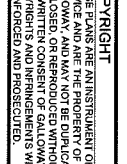
**SECTION W-W: PERPENDICULAR RAMP**

**SECTION X-X: PERPENDICULAR RAMP**

**SECTION Y-Y: PERPENDICULAR RAMP**

**SECTION Z-Z: PERPENDICULAR RAMP**





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## R5.4





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PROPOSED MAJOR CONTOUR  
PROPOSED MINOR CONTOUR  
EXISTING MAJOR CONTOUR  
EXISTING MINOR CONTOUR  
EXISTING STORM SEWER  
PROPOSED STORM SEWER  
PROPOSED STORM INLET  
PROPOSED RIGHT-OF-WAY  
FUTURE RIGHT-OF-WAY  
PROPOSED LOTLINE  
FUTURE LOTLINE  
EASEMENT LINE



A circular diagram representing a subbasin. The top half of the circle contains the number "200". The bottom half is divided into two equal vertical sections. The left section contains the values "0.20" and "0.50" stacked vertically. The right section contains the values "98" and "acres" stacked vertically. Four arrows point from text labels to the circle: "SUBBASIN ID" points to the top half; "SUBBASIN AREA (ACRES)" points to the right half; "5-YEAR RUNOFF COEFFICIENT" points to the "0.20" value; and "100-YEAR RUNOFF COEFFICIENT" points to the "0.50" value.



DESIGN POINT

OSP SYMM SUBBASIN BOUNDARY

PROJECT SPECIFIC SUBBASIN BOUNDARY

OVERLAND FLOW DIRECTION  
(I.E., LANDSCAPING)

DIRECT FLOW DIRECTION- FUTURE  
(I.E., PAVEMENT, CURB AND GUTTER)

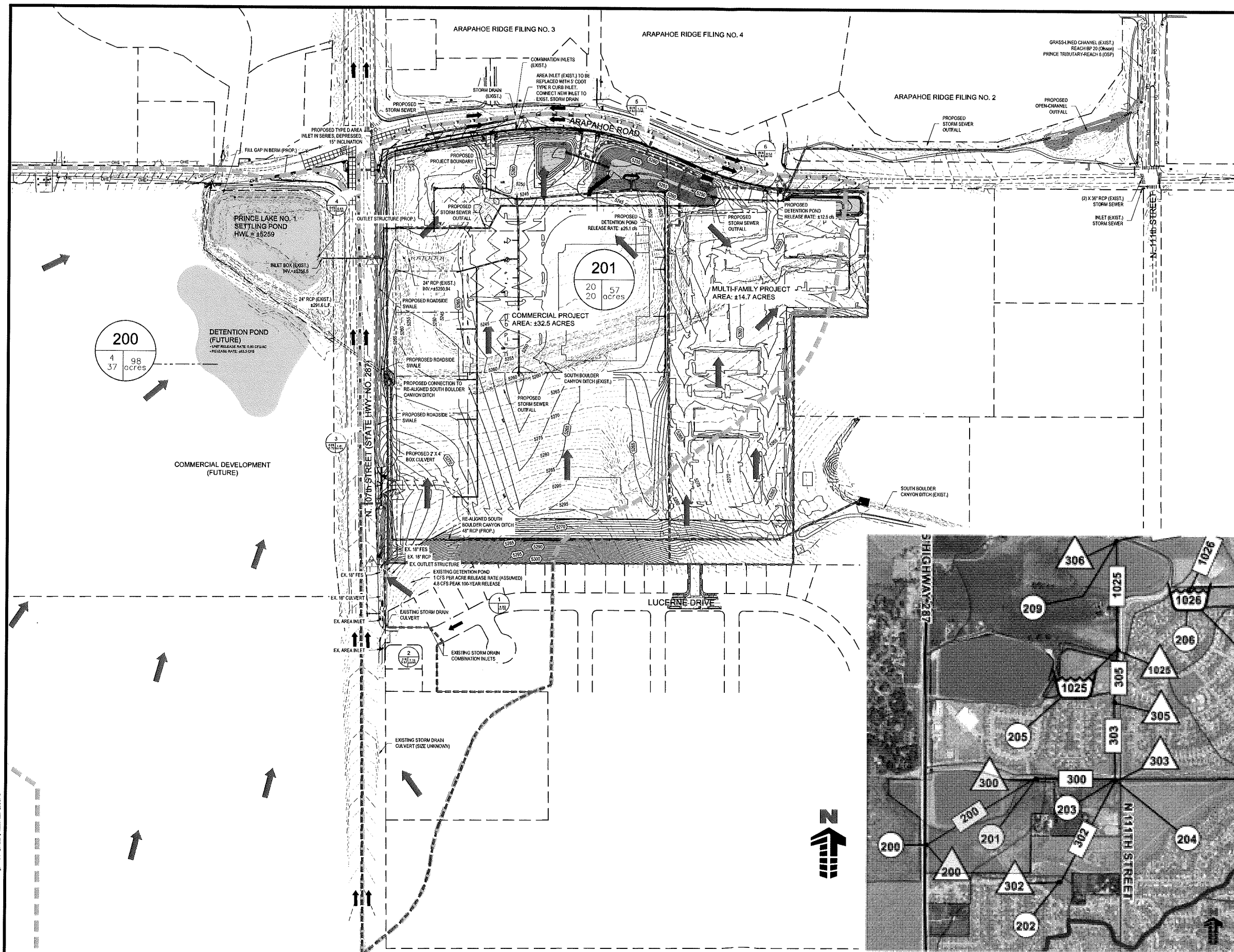
1 DETENTION POND FOOTPRINT (PROPOSED CONDITION)

Project No:		
Drawn By:	HHF	05/17/2017
Checked By:	HHF	

**NINE MILE CORNER**  
ERIE, COLORADO  
N. 107th Street and Arapahoe Road

DR01

1 of 1



**Inset from Figure B-6**  
**Town of Erie Outfall Systems Plan (West of Coal Creek)**  
**RESPEC Consulting & Services, January 2014**

SCALE - NTS

# FINAL DRAINAGE PLAN

## NINE MILE CORNER | ERIE, CO

SCALE - 1" = 150'



# STATE DEPARTMENT OF HIGHWAYS

## DIVISION OF HIGHWAYS—STATE OF COLORADO

## CONVENTIONAL SIGNS

County Line  
Township or Range Line  
Land Lines  
Property or Tract Line  
City Limits  
Railroad  
Existing Road  
New Road

Control of Access  
Access denied by Dead End  
Right of Way Line  
Protected by Freeway (Virgin Location)  
Top of Cuts  
Toe of Fills  
Barbed Wire Fence  
Chain Link Fence  
Woven Wire—Combination Fence  
Snow Fence  
Wood Fence  
Deer Fence

Trees  
Deciduous  
Coniferous

Te. & Tel. Lines  
Electric Lines  
Elec. Trans. Tower  
Buried Tel. Cable  
Buried Electric Cable  
Gas Main  
Oil Main  
Water Main  
Sanitary Sewers  
Storm Sewers

Road Approaches  
Section Corner  
Center of Section

Culverts & Drains  
Proposed  
Existing  
Proposed  
Existing (To be Dashed)

Bridges  
Existing (To be Dashed)

### PLAN AND PROFILE OF PROPOSED FEDERAL AID PROJECT NO. FC-HES 007-2(5) STATE HIGHWAY NO 7 & 287 BOULDER COUNTY

## RIGHT OF WAY

SCALE OF ORIGINAL DRAWINGS

PLAN SHEETS 1" = 50'

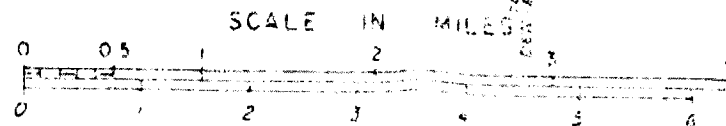
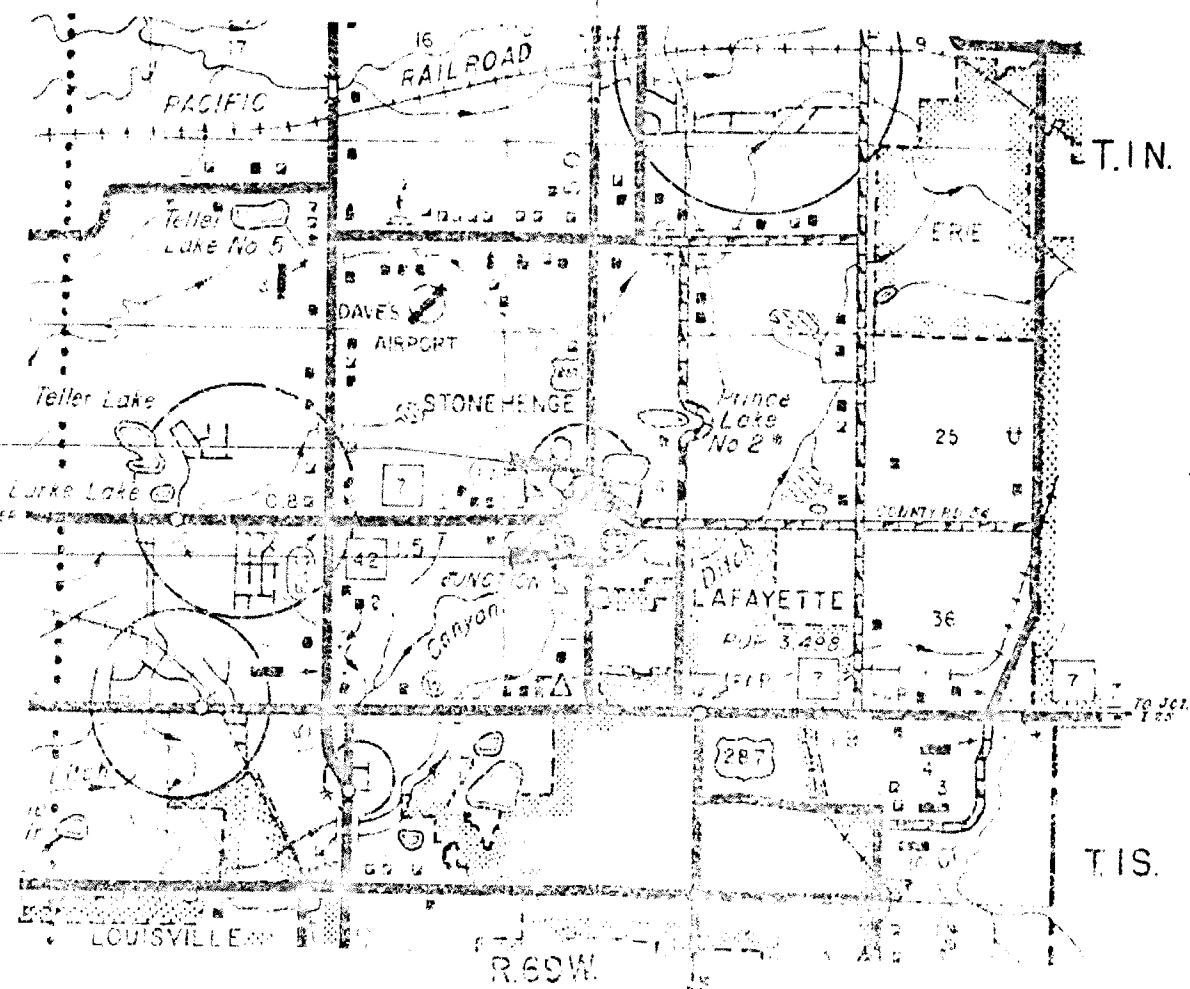
DETAIL SHEETS 1" = 10'

OWNERSHIP MAPS 1" = 200'

ROW LENGTH OF PROJECT = 0.363 MILES

BEGIN ROW PROJECT FC-HES 007-2(5)  
STATION 516+46.2 (Proj. S.H. 287)

END ROW PROJECT FC-HES 007-2(5)  
STATION 535+44.2 (Proj. S.H. 287)



SCALE IN FEET

PROJECT NO.	SECTION	SHEET NO.	TOTAL SHEETS
FC-HES 007-2(5)			
RIGHT OF WAY			
SHEET NO. 7 & S.H. 287			

## INDEX OF SHEETS

1 TITLE SHEET

2-2A TABULATION OF PROPERTIES

3-7 PLAN SHEETS

8 OWNERSHIP MAP

DIVISION OF HIGHWAYS	
APPROVED	
SUPV. ENGINEER	DATE

DEPARTMENT OF TRANSPORTATION FEDERAL HIGHWAY ADMINISTRATION	
APPROVED	
DIVISION OF HIGHWAYS	

R.O.W. TABULATION OF PROPERTIES IN BOULDER

COUNTY

S.H. NO. 7 & 287

RIGHT OF WAY

Jct. SH 7 & SH. 287

PROJECT NO.	FC-HES 007-2(5)	SHEET NO.	2
-------------	-----------------	-----------	---

PARCEL NO	OWNER	ADDRESS	LOCATION	AREA IN ACRES				PARCEL NO	REMARKS	REVISIONS		
				PARCEL	TO BE ACQUIRED	REMAINDER				1-4-83	1-4-85	
						LEFT	RIGHT			Del. Par. 1, TE-7	D-4	
										Name Chg. 5, TE-5	D-4	
			T.1N., R.69W., 6TH PM.									
1	EARL GEOFFREY MORRIS	10869 ARAPAHOE RD.	S <sup>2</sup> SE <sup>4</sup> SEC. 27	1.116	1.116	See Par. 3		1				
DEL.	TO	LAFAYETTE, COLO. 80026										
	TOWN OF ERIE											
2	MARTIN H. JANULE & BETTY FREE JANULE JT.	1161 55TH ST. BOULDER, COLO. 80303	SE <sup>4</sup> SW <sup>4</sup> SEC. 27	0.084	0.084	-0-	-0-	2				
3	EARL GEOFFREY MORRIS	10869 ARAPAHOE RD.	SW <sup>4</sup> SE <sup>4</sup> SEC. 27	0.267	0.267	72±		3				
	TO	LAFAYETTE, COLO. 80026										
	COUNTY OF BOULDER											
4	GARY D. CONNELLY & JANET P. CONNELLY JT.	9572 SCHLAGEL DR. LONGMONT, COLO. 80501	SE <sup>4</sup> SW <sup>4</sup> SEC. 27	0.491	0.491		0.553	4				
5	DAVID ALLEN BLAKESLEE	10695 ARAPAHOE LAFAYETTE, COLO. 80026	SE <sup>4</sup> SW <sup>4</sup> SEC. 27	0.142	0.142		1.6±	5				
TE-5	DAVID ALLEN BLAKESLEE	10695 ARAPAHOE LAFAYETTE, COLO. 80026	SE <sup>4</sup> SW <sup>4</sup> SEC. 27	0.072	0.072				TE-5 DRIVEWAY RELOCATION			
6A	TOWN OF ERIE		NE <sup>4</sup> NW <sup>4</sup> SEC. 34	0.324	0.258	3.4±		6A				
6B	TOWN OF ERIE		NE <sup>4</sup> NW <sup>4</sup> SEC. 34	0.275	0.275	See Par. 6A		6B				
6C	TOWN OF ERIE		NW <sup>4</sup> NE <sup>4</sup> SEC. 34	0.738	0.738	17±		6C				
6D	TOWN OF ERIE		SW <sup>4</sup> SE <sup>4</sup> SEC. 27	0.274	0.274	See Par. 6C		6D				
TE-7	TOWN OF ERIE		S <sup>2</sup> SE <sup>4</sup> SEC. 27	2.013	2.013				TE-7 RESERVOIR ENLARGEMENT			
DEL.												

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TIN, R 69W, 6TH PIA

REVISIONS	DATE	BY	APP'D
1	11/10/10	TC	TC
2	11/10/10	TC	TC
3	11/10/10	TC	TC
4	11/10/10	TC	TC
5	11/10/10	TC	TC

RIGHT OF WAY  
Sec 27 & 28 SH 287

① EAP. J. FERRY MORRIS  
SILVER TOWN OF ERIE

⑦ TOWN OF ERIE  
ILLINOIS

UTILITY OWNERS  
Mountain Bell  
Public Service  
Left Hand Water District

SE 1/4 SEC 27  
TIN R69W

ERIE  
LAKE

STORAGE POND  
DOMESTIC WATER SUPPLY

Projected BL  
 $\Delta = 15^\circ 30' 11''$   
 $D = 6^\circ 00'$   
 $T = 130.0$   
 $L = 238.3$   
 $R = 954.9$

Sta 516+51.3 (Proj. SH 287)  
Sta 110+00.4 (Proj. BL)

Parcel 1  
S 5 1/4 corner Sec 27  
to S 1/4 corner  
Pine Rowles  
Pine Rowles

505

510

515

520

10 LOMMONT US 287

2 GAS LINE PUBLIC SERVICE

6" WATER LINE LEFT HAND WATER DIST  
MOUNTAIN BELL TELEPHONE

18" x 17" STEEL PIPE  
IN DITCH

TEL PLO  
STAND PLO

18" x 50" CMP  
w/ 18" BOLLERS  
15" x 20" CMP SLOPE DRAIN  
w/ 18" BOLLERS

15" x 16" BOLLER TUBE  
SLOPE DRAIN  
15" x 16" CMP  
15" x 16" BOLLER TUBE  
SLOPE DRAIN

18" x 50" CMP  
w/ 18" BOLLERS (HAWKSPR)

SW 1/4 SEC 27  
TIN R69W

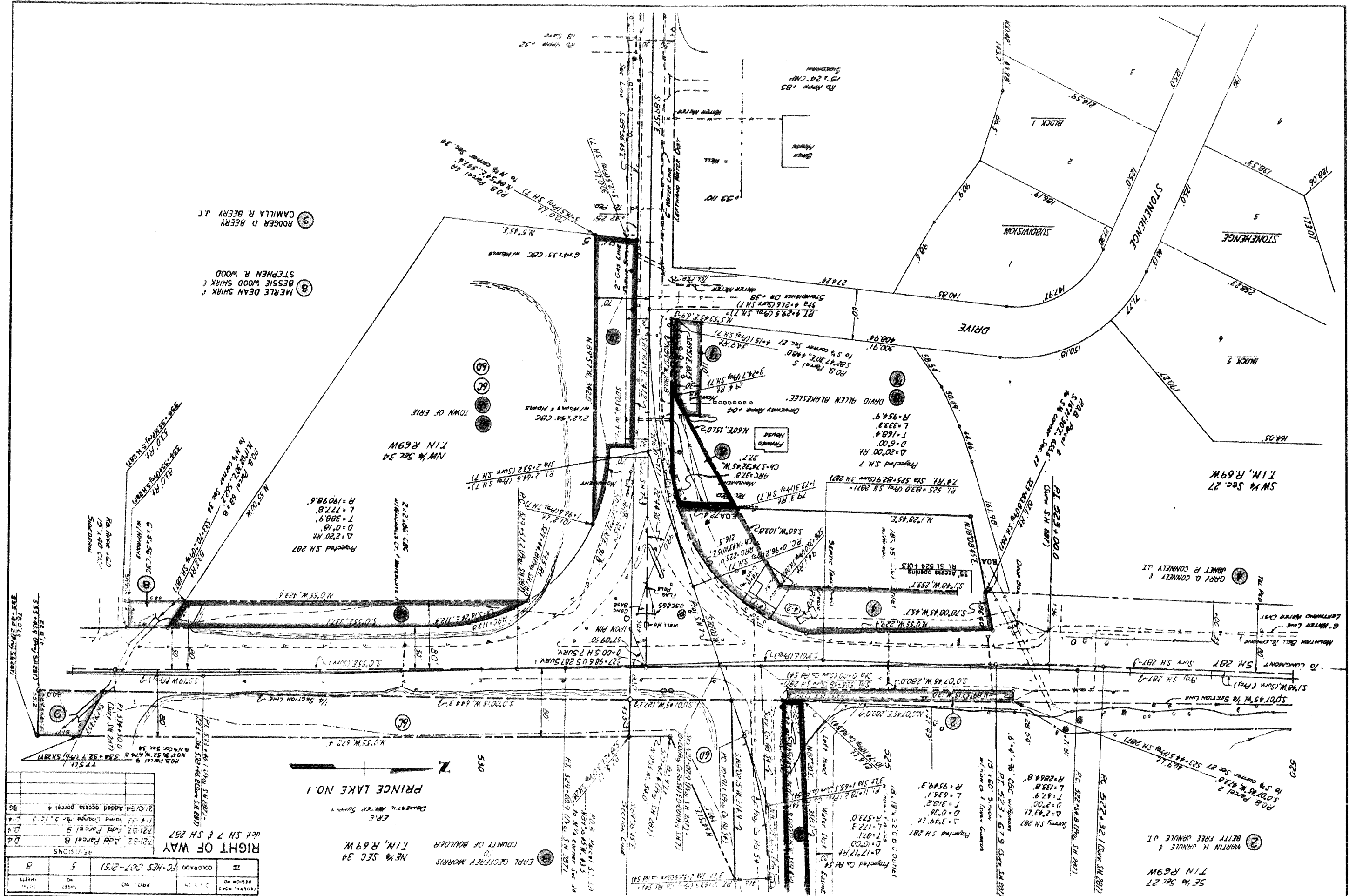
STONEHENGE

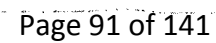
SUBDIVISION

STONEHENGE CIRCLE

STILLWATER





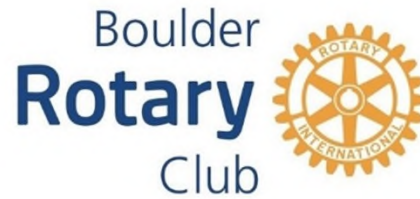






**RIGHT OF WAY**  
Jct. S.H. 7 & S.H. 267  
OWNERSHIP MAP





## **The Future of the Pillars at Nine Mile Corner Lafayette YMCA – April 12, 2022**

### **Panelists**

Bill Meyer – Boulder Rotary Club

Jason Marmor – Colorado Department of Transportation

Denise Grimm – Boulder County

**For more information, see [9milecornerpillars.com](http://9milecornerpillars.com).**

### **Evaluating the Desirability of Pillar Alternatives**

**Location** – The original monument was intentionally located at the “entrance” to the City of Boulder

- How important is this location?
- Is the alternative location comparable or otherwise acceptable?

**Function** – The original monument was intended to flank Arapahoe Road and form a visual Gateway

- How important is it that the monument serve as a gateway?
- Will the alternative fulfill this function?

**Purpose** – The monument was intended to honor soldiers from Boulder County who served in WWI

- Has the original location fulfilled that purpose?
- Will the alternative adequately fulfill this purpose?

## **Evaluating the Feasibility of Pillar Alternatives**

Ease of Site acquisition (purchase, donation, public space)

Ability to obtain Zoning/Permits

Ease of public access and safety (parking, bike or walking paths, conflicts with traffic)

Vehicular safety

Permanency (likelihood of future redevelopment)

Responsibility for future maintenance

Potential for vandalism

Potential costs:

- Repair/reconstruction of current location

- Site acquisition costs at new location

- Design costs

- Site preparation costs at new location (surveying, grading, sidewalks, entrances, parking, utilities)

- Legal/administrative costs (site acquisition, permitting)

- Restoration/relocation expenses

- Amenities (signage, landscaping, lighting)

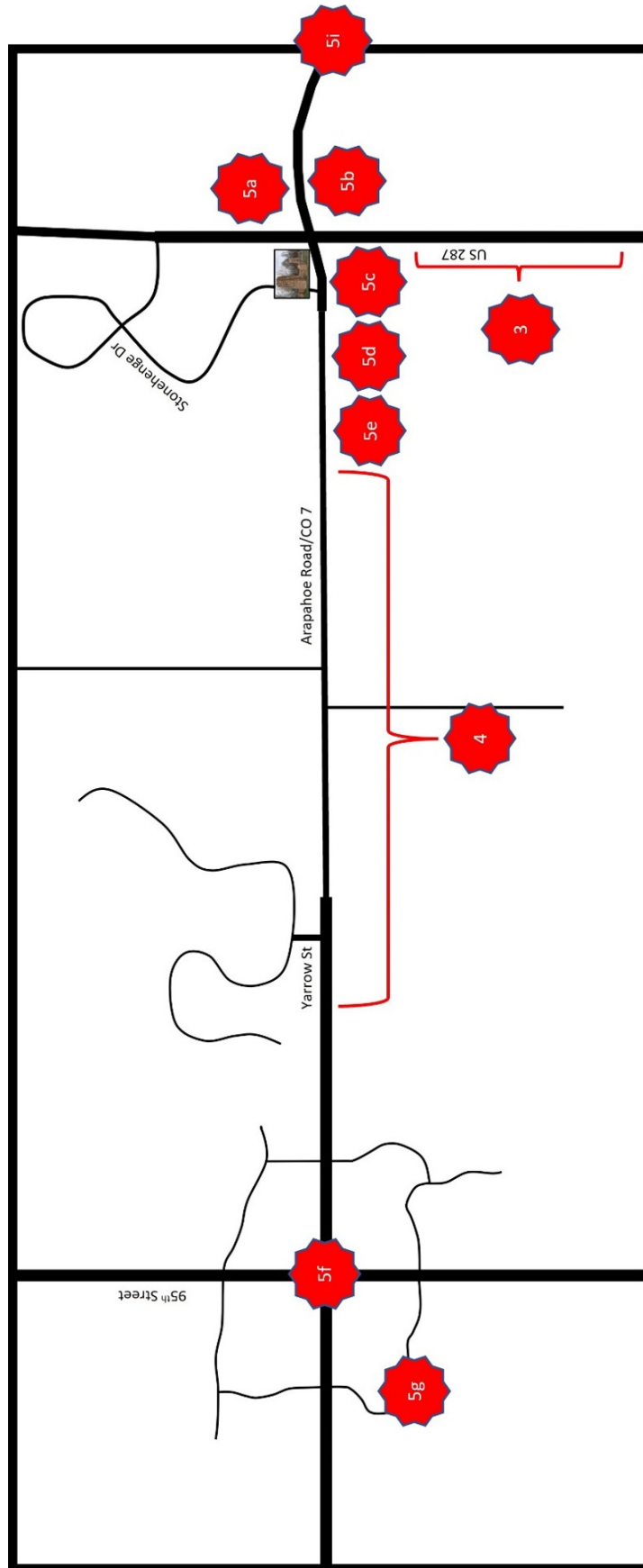
Funding

- Current costs

- Future Maintenance

## **PROPOSED ALTERNATIVES**

1. Demolish pillars
2. Leave pillars in place and:
  - a. Do nothing
  - b. Restore and maintain
  - c. Restore, maintain, and enhance with signage, etc.
3. Relocate pillars to flank US 287 south of Arapahoe
4. Relocate pillars to flank Arapahoe between current location and 95th Street
5. Relocate both pillars to single spot (*i.e.*, not flanking a roadway) in the vicinity of the current location
  - a. NE corner of Arapahoe and US 287 (Arapahoe Ridge)
  - b. Nine Mile Corner Development
  - c. Erie property immediately to the south (old Prince Reservoir No. 1)
  - d. Tebo Development
  - e. Silo Development
  - f. Intersection of Arapahoe and 95<sup>th</sup> St.
  - h. Lafayette YMCA
  - i. Intersection of Arapahoe and 111<sup>th</sup> St.
6. Relocate pillars to another spot away from current location
  - a. Flanking entrance to Legion Park
  - b. Flanking entrance to US 36 overlook
  - c. Flanking walkway or roadway on CU campus
  - d. Flanking entrance to Flagstaff Mountain Park
  - e. Entrance to an open space location
  - f. Boulder Mall
  - g. Infinite Walk of Peace (adjacent to Boulder Public Library)



## Potential Nearby Relocation Sites

## **THE FUTURE OF THE GATEWAY PILLARS - WHERE WE ARE NOW**

**(April 18, 2022)**

After the public forum on April 12, and the deluge of emails and posts that we received, the issues are coming into sharper focus. Here is where we are at the moment:

**Unanimity** – It’s fair to say that the unanimous view of everyone who has offered an opinion is that the pillars should be preserved in some form. Destruction and removal are not options.

**Consensus** – While there is no consensus on some issues, several points of general consensus have emerged:

1. The pillars should be located on a site where they will not again be subject to further movement or disturbance.

2. Any relocation must be to a governmentally owned space because:

- a. Ownership by a governmental entity is needed to provide perpetual maintenance.
- b. The monument should not become identified as part of a commercial enterprise.

**NOTE:** While it has been suggested a private landowner might be persuaded to donate/sell a small parcel to a governmental entity for the pillars, it seems extremely unlikely that a governmental entity would take on the responsibility for a small parcel that is not attached to a larger parcel owned by that entity.

3. Any solution should enhance the pillars’ visibility as a memorial.

4. The pillars should be moved in tandem; moving one pillar alone is not an option.

**Preferences** – Though perhaps not rising to the level of requirements, a few majority preferences are apparent:

1. *The pillars should be located at a site as near as possible to the current location.* While the builders’ original hope of using the Gateway to attract tourists to Boulder has long since disappeared, the pillars over the decades have become a sentimental landmark for Boulder County residents. If possible, a relocation site should recognize that sentiment and keep the pillars where they will continue to be easily and routinely enjoyed by Boulder County residents.

2. *If relocated, some semblance of a “gateway” appearance should be maintained.* The pillars were never designed as a stand-alone monument, but instead were intended to be integrated along with the canon park into the Road of Remembrance. While the Road of Remembrance cannot now be recreated, any relocation should attempt to maintain some of that character.

3. *The pillars should have reasonable visibility for persons in vehicles passing by on a nearby highway or street.* While there has been discussion about providing for pedestrian access to the pillars, the pillars' original setting was not pedestrian friendly. Nor were the pillars designed with features (e.g., sculptures, mosaics, dioramas) to attract close examination by foot traffic. Instead, the Gateway was specifically designed to be viewed from passing vehicles; even the signage (plaque, cornerstone) was located for viewing from a vehicle passing between the pillars.

Using these parameters, the list of potential alternatives can be narrowed, leaving the following list (in order of distance from present location):

1. *Leave the pillars in place, but repair and enhance.* For some, keeping the pillars in their current location remains paramount. If that option is chosen, the clear consensus is to repair the pillars and find some way with signage to promote their history and memorial purpose.

2. *Reconfigure the intersection to better protect and display the pillars.* Several persons argued that the intersection should be rebuilt, both to protect the pillars and improve traffic safety.

3. *Erie property on the southwest corner of the intersection (old Prince Reservoir No. 2).* Within the parameters, but not discussed at the forum.

4. *Proposed transit superstation on US 287.* Though not discussed in depth at the public forum, relocating the pillars to the proposed transit superstation on the west side of US 287 south of the intersection fits within the parameters thus far identified.

5. *Erie Lake.* Not on the prior list, Erie Lake (located north of the intersection on the east side of US 287) fits the parameters.

6. *Future intersection of Arapahoe and Aspen Ridge Drive (entrance for Silo and Tebo developments).* Within the parameters, but not discussed at the forum.

7. *Bullhead Gulch – Yarrow Park.* Located on the north side of Arapahoe west of the current location, these areas are owned by the City of Lafayette. Not previously discussed.

8. *Forest Park.* Small park owned by the City of Lafayette on the southeast corner of Arapaho and 95<sup>th</sup>.

9. *Legion Park.* County owned property west of 75<sup>th</sup> Street.

10. *Davidson Mesa Scenic Overlook on US 36.* CDOT-owned.

Assuming no more serious alternatives emerge, the next step is to start exploring the feasibility of the various options. While we will be gathering information on all of the listed alternatives, anyone with an interest in a particular location is encouraged to do their own research and thinking, and offer a more detailed outline of a plan for that location.

Bill Meyer

MESSAGES RECEIVED CONCERNING GATEWAY PILLARS  
(April 11, 2022 as of 8:30 am)

From: Guy Higgins <guy1016@icloud.com>  
Date: Mon, Mar 14, 2022 at 11:10 AM  
Subject: 9 Mile Corner Memorial

I am a veteran of 30 years active duty in the US Navy. My father was also a 30-year vet, my mother was a WWII Navy nurse, and my brother was a career Hospital Corpsman. Memorials to our veterans are important to me. I attended the presentation at the Lafayette library on Sunday the 13th. I very much appreciated it. I will be unable to attend the session on April 12th since I already have two conflicting meetings that evening.

That said, I think that preserving the memorial pillars should be a high priority for the city and the community. Lafayette developed a Comprehensive Master Plan over the last two years, and in that CMP are proposals for visually impactful "Gateways" to the city. I think that one of those gateways should be park with the pillars moved from their current location (where I think that they are being gradually destroyed by passing traffic) to a gateway park — a park that could be themed as a memorial park for all those Americans who fought in our wars since Lafayette was founded. The city cemetery is the resting place for some small number of those veterans dating back to the Civil War.

I would be stunned if Arapahoe Road isn't widened within the next decade. The traffic to and from Boulder from the surrounding municipalities is increasing every year and I don't see that ending. Lafayette, Erie, Thornton, even Brighton are going to provide housing for people working in Boulder, so improvements to the transportation in/out of Boulder will be necessary. That means that Arapahoe will have to become four lanes - two in each direction, and that means that the Arapahoe - 287 intersection is going to get a whole lot bigger, endangering the pillars. Let's bite the bullet now and move them out of any potential eight-lane intersection (two travel and two turn lanes in each direction).

Very Respectfully,  
Guy Higgins  
CAPT, USN (Ret)  
Lafayette, CO  
714-276-3100



Date: Thu, Mar 24, 2022 at 3:53 PM

Subject: New submission on Collected Forms "Collected form: .et\_pb\_contact\_form, .clearfix"

To: <chad@tmcdigitalmedia.com>

Email:

kaystoakes@comcast.net

First Name:

Kay

Last Name:

Stoakes

Re: Nine Mile Corner Program. My mother, Virginia Ostrander was a 100-year resident of Boulder County, 1918-2018. As children she would often tell us about various landmarks and things unique to Boulder County. Because many family members were scattered around the County we did a fare amount of riding most the country roads (by automobile). Anyway she always said the Nine Mile Pillars were built by CCC during the 1930's as were many other improvements....Flagstaff road barriers, theatre, picnic areas and shelters, Bluebell Canyon shelter, Boulder Canyon Road barriers, among a few. She also told us the Nine Mile Corner name was because from the pillar location is s 9 miles to Boulder, 9 miles to Longmont and 9 miles to Louisville. Questions, my land line 720-600-5193

Mel Haik <mel.haik@icloud.com>

April 10 11:09 AM

Please save those important, historic pillars. If they have to be moved to save them, then move and reconstruct them on Arapaho to the west. If a WWI cannon can be acquired, please add that armament. We must honor those who served and we must remember the sacrifices.

Thanks!

Posted to Next Door as of 8:30 am on April 11

Debbie Sencenbaugh • Old Hwy 52 and WC- 5

Promises were made at the time, by my recollection, that there would be \*no problem\* preserving the pillars without relocating them. Government promises are easily broken. CDOT's as well. I'm jaded.

Mary Anne Hubble • Monarch Estates - Firestone

I remember the same discussions and promises. Although I thought they moved them? I wonder if there is a way to research articles in the Daily Camera? Do you have any idea when the road changes were made?

Ron Brotzman • Niwot

Don't destroy them. They were bombed in 1966 by a bunch of radicals. The canons had to be removed then They were never the same. They honor our WW1 vets. Keep the history. Who designed this new road anyway. Boulder county commissioners must not have been interested in what happened to them.

Jean Dayhoff • Brennan by the Lake

Everyone should attend or voice their opinion!!

Jackie Connor • Erie Commons

yes, attendance is needed. But, come armed with facts. Knowledge is power!

Dave Svoboda • Longs Peak

To the contrary, most people don't care so much about facts as they do feelz.

Jackie Connor • Erie Commons

As Past Chairman of the Erie Historic Preservation Advisory Board, we did extensive research on them. They do mark from their point to the City of Boulder. Thus, they are known as the Nine Mile Markers. Tyler Carlson, who is the developer of Evergreen is willing to have them moved to the new area where Lowe's is being built. He respects history and wants to see them preserved. They can be moved by wrapping them. Feel free to contact me at: [classyjackie93@gmail.com](mailto:classyjackie93@gmail.com).

Suzanne Cote • Phillips/Owl/GR/Dawson

Thanks for letting us know. By wrapping them with what? Would it be miles of plastic, and /or styrofoam, wood? I have seen this in a shop in action, as they had to ship their high end custom cabinetry to home owners in Aspen and other hospitality places with endless wasteful resources. It was jaw dropping! This would produce a byproduct of a mountain full of plastic waste. Never mind the relocation program of the beautiful, well placed pillars. Road redesign and implementation for a whole different feeling. It would be awful and consequential!

Debbie Sencenbaugh • Old Hwy 52 and WC- 5

A few years ago we attended a Frederick meeting about the illegal flagpole annex of parts of CR5. It was contentious, with a large number of affected Weld residents attending wishing to speak. Midway, the City Attorney, thinking his mike was off, which it wasn't, advised the Mayor "by law we have to let them speak, but we don't have to listen to them" (quote not verbatim) Don't have the date but you can look up the meeting re: CR5 annexation. Don't know if it made the record as we were clearly not supposed to see it

Keri Beard • Erie Village

This is an area landmark and it would be a shame to lose it. 9 Mile is an eyesore and I wish they'd never built it.

Jackie Connor • Erie Commons

Reply to Keri: Beard how sad.

Jackie Connor • Erie Commons

Keri Beard the 9 Mile Pillars have never been Landmarked. This has to be done by the owner requesting it through the Town/City or County to the local Historic Advisory Board/Commission. The Erie Historic Preservation Advisory Board is trying to dig out the records for ownership.

Jackie Connor • Erie Commons

the 9 Mile Pillars have never been Landmarked. This has to be done by the owner requesting it through the Town/City or County to the local Historic Advisory Board/Commission. The Erie Historic Preservation Advisory Board is trying to dig out the records for ownership.

Gloria Davis • John Breaux

I couldn't agree more Keri - thank you for standing up!!!

Suzanne Cote • Phillips/Owl/GR/Dawson

Those pillars are an extraordinary piece of history, and a beautiful element! Why must we argue about such obviously treasured things that developers want to complain about? They need to be preserved!

Caroline Tyra • Erie Commons

The 'serious vehicular hazard' was not a thing before development at the corner. So what is the real deal here?

Mary Anne Hubble • Monarch Estates - Firestone

Reply to Caroline Tyra We've recently traveled through that intersection a couple times and, if traveling the speed limit, I do not see why they would be an impediment to traffic. The traffic was really heavy when we went through....I wonder if there is a hidden agenda like future plans to redo the entire intersection?

Jackie Connor • Erie Commons

Reply to Mary Ann Hubble. You nailed it! Future plans are to reconfigure the intersection.

Mary Anne Hubble • Monarch Estates - Firestone

Sneaky aren't they?!

Tiffany L. • Longmont Estates East

Overcrowding destroying more history.

Jackie Connor • Erie Commons

Wait a minute! Have you been to Italy or Germany?

Sue Peterson • Gay St

My dad told me it used to have an arch.

Jackie Connor • Erie Commons

and a Cannon.

Will Holsclaw • Gunbarrel Estates

I believe they had to remove the arch when they moved them further apart several decades ago to accommodate the wider roadway

Ronda Romero • Indian Peaks South

I remember as a child the arch had green ivy (?) growing on it. We would drive under it as we went to Boulder.

Gloria Davis • John Breaux

yes it did, my brother-in-law's Grandfather helped build those arches!!

Lee Hazzard • Isabelle-Arapahoe

It's not the pillars creating the vehicle hazard it's the design of the merges and lane turns! Pillars should remain in place! Enough destroying our history and preservation. Enough sprawl!

Gloria Davis • John Breaux

hear hear Lee!!! Or maybe people could just be a little for respectful instead of thinking these roads were built just strictly for them!!! Love one another!!

Randy/Deb Coffin • Arapahoe Ridge

The merge lanes are an insane way of managing traffic! The merge lanes on Arapahoe east and west of US 287 are dangerous! I have been nearly hit a few times by drivers who appear not to check the lane they are entering before merging! Who designs stuff like that and what is their design rationale!?

Barbara Hansen • Blue Heron

agree. It's always a race to beat out cars in the other lane to get ahead. Same issue at 95th and Baseline.

Jim MacDonald • Kenosha Farm

It's way more important to be checking or talking on your cell phone than to be paying attention to all the 3-4000 lb. missiles all around you. Safety? WE don't need no stinking safety

Richard E. • Niwot

Cell phone addicted people have a death wish these days. Just try crossing the street doing that in New York City. Road runner anyone?

christy gorringe • Niwot

dang there goes another iconic landmark. don't we love progress...speechless.

Rhonda Pollock • Countryside

Well said christy

Jim Wester • East Weld

I hope they can safely relocate them. They are too close together for how busy the intersection is.

Molly Sabatino • Colliers Hill

I've always wondered about those pillars. Thank you for the history. I would be sad/mad if they were destroyed.

Steven Dunbar • Indian Peaks North

Please read William Meyers historical summary on these pillars, link below.

While these were built to honor those that served in WW-I, make no mistake that it was commercial and tourism interests that pushed their construction along from the outset- the same types of interests that are developing the land around it now.

- \* The pillars were not maintained or cared for at all for a solid 50 year span from the 30's to the 80's.

- \* The north pillar was accidentally destroyed when attempting to move on a prior occasion to widen the road, and was rebuilt where it sits today, sitting in brush and shrubs, so it already isn't "original". Only the south pillar inside the pork chop traffic island is as built, in its original location.

- \* Legion park in east Boulder is tied loosely to the pillars as well. Maybe they should be relocated to Legion Park.

- \* CDOT appears to not have followed the historical preservation rules on this, but in the long run, that probably wouldn't have mattered- they would still have to move most likely given the immense pressure from traffic and development.

The fact is the growth of the area demands they be removed one way or another. I hope they can be moved safely to a prominent, meaningful location nearby or to Legion park, and that their original

purpose of honoring veterans of WWI can be better accomplished. As is and has been for a very long time, the pillars are completely inaccessible and unable to serve this purpose.

<https://boulderrotary.org/wp-content/uploads/2022/03/Doc.-A-History-and-Current-Status-of-Nine-Mile-Corner-Monument.pdf>

Caroline Tyra • Erie Commons

why should they be moved? Location is an important part of the history. Moving them to a park dilutes their purpose as meaningful points on a map.

Robert Greenhalgh • Dakota Ridge

I've always wondered what those were. They definitely should be saved!

Cary Teegarden • South Near Rec Center

Keep the pillars where they are. Hire some engineers that know what they're doing, we must stop changing history. And while you're at it you could re-restore the sign that used to be north of Iris arch over the road saying welcome to Boulder coming in and you're now leaving Boulder. They tore that down years ago. They're ruining the historic icons of the city. I have family dating back to the 1800s I am fourth generation we now have six generation Boulder and these people that are moved in here from the coast have changed Boulder drastically some for the good And some not so good.

Sally Kaplan • Wildgrass

Totally agree. Landmarks and memorials are exactly that! Sally Kaplan

Donna Deffke • SW Longmont Estates

My mother used to go by them when she traveled from Denver to Boulder to school at CU. The Nine Mile Corner intersection was the only way to get to Boulder from Denver at the point in time. Most the folks that remember that fact are long buried now.

Richard B., Country Fields

think they should remain where they are and be reconditioned.

Jenny Ott • Flatiron Meadows

Why would they propose moving the pillars now, after they recently did so much roadwork??? Nothing like build a road, ripping it out, and building it again. How many times can we do that in Lafayette or Erie?! It's seems there is a game to constantly throw money away through roadwork around here.

John Putnins • Erie Village

hey are thinking about moving the pillars now because they forgot about them when they planned the enlarged intersection. People don't pay attention to details anymore.

Jenny Ott • Flatiron Meadows

thanks for the information. Wow, that's a pretty big oversight 🙄 I can't imagine I'd have a job if I had that BIG of oversight!

Don Parcher • Heritage Park

As I recall, there's a plaque that refers to the World War or the Great War, it being before World War II.

Richard B. • Country Fields

Look at prior post's, they explain all about the pillars

Don Parcher • Heritage Park

I just think it's interesting that it refers to "the World War" instead of "World War I"

<https://boulderrotary.org/wp-content/uploads/2021/12/Plaque.jpg>

John Rhinehart • Longmont Estates East

I remember those pillars, seeing them for the first time when my husband and I were house-hunting prior to relocating here. It seem it also had an arch across Arapahoe connecting both pillars on either side of the road. I asked our realtor about them and she told me they were a "war memorial."

Boni Bichler-Lagana • Godding Hollow

There used to be canons there and my Grandpa would be the one that kept them polished. He would take me with him when he went. He was a WWI disabled Vet. That area means a lot to my family.

Paul Donaghey, Historic East Side

They should be relocated to an area of honor at any one of our county buildings or libraries. In a new location they might actually be able to be viewed & appreciated.

Demi Prentiss • E. South Boulder Rd.

Happy to hear about Boulder Rotary's constructive involvement in mitigating a bad situation.

The presentation @ 9milecornerpillars.com is about 40 min long and worth the time.

Vic Hoerner • Niwot

It is so easy to tear history down. Lets try to honor the past.

Donna Caccamise • Isabelle-Arapahoe

These pillars were compromised as part of recent development, which included some really poor engineering in changing that corner. Where was the oversight? I live nearby and never got a notice about any of these new developments on Arapahoe at and near that corner during their planning stages. There is a host of snafus this development has precipitated that did not get proper consideration during planning.

Wes Nelson • East Weld

I've lived in this area for more than 50 years and never knew that there was an actual significance to those towers. Thanks for the info.

Serin Silva • Orchard Creek

Well, the city council just wants to rip everything out so I'm not surprised. Forget history, just sell, rip and take out anything that has any meaning.

Kelly Keatley • Orchard Grove

Yes Please Keep Our History Safe & Alive!us P.S. Loving Memories of Seeing the Lakes & Pillars on our way going to Denver versus going on the Turnpike

Shirley Parry • Old Town

I will never understand why people find the need to destroy historical objects. The pillars would be meaningless if relocated. Remember the beautiful huge cottonwoods that were torn down on Arapahoe just to widened the road? Stop it already...

Francine Startup • Winding Trail

Yes! When I moved here to Lafayette in 1988 from The east cost Hudson Valley. Worked next to Washington's headquarters .. I was amazed at the lack of historical interest here and things have not changed! So sad.

Vickie Slade • Bullhead Gulch/Spring Creek

So sad to see all the commercial development at this intersection where there once was a lake. Erie is literally adding developments everywhere, gobbling up land, adding rooftops and exacerbating traffic issues. Lafayette seems to be trying to compete for tax dollars and adding more and more commercial development along the 287 corridor. Stop it already!!



Date	Estimate #
4/11/2022	4977

3060 Walnut St. Denver, Colorado 80205  
303-297-2004  
FAX 303-296-2840  
www.brsrestores.com

Name / Address					
Colorado Department Of Transportation 10601 10th Street Greeley, CO 80634		Terms		Project	
		Pricing is Good for 30 days		Road of Remembrance Memorials	
Item	Description	Qty	Rate	Total	
1	<p>Road of Remembrance Memorials</p> <p>OPTION B Crane picking and moving stone pillars in complete sections.</p> <p>Rebuilt Pillar - Disassemble each buttress wing wall, document salvaged stone as well as possible, clean mortar debris from existing stone and palletize for relocation. Salvage precast elements for molding and manufacturing matching replacements. To divide each pillar portion of the monument into 3 sections by "slicing" horizontally and inserting steel channels sandwiched with through bolts and install bracing at interior. Crane lift sections off and truck to new location. Off load for later assembly. At new foundation, Install each stone pillar section and restore shoring damage and natural weathering damage. Rebuild stone at buttress wing walls as per original design and install new precast caps. Cut in and modify for three window glass panes that currently are not present.</p> <p>Historic Original Pillar - Disassemble each buttress wing wall, document salvaged stone as well as possible, clean mortar debris from existing stone and palletize for relocation. Salvage precast elements for molding and manufacturing matching replacements. To divide each pillar portion of the monument into 3 sections by "slicing" horizontally and inserting steel channels sandwiched with through bolts and install bracing at interior. Remove stone and concrete top, salvage stone for reinstallation. Crane lift sections off and truck to new location. Off load for later assembly. At new foundation, Install each stone pillar section and restore shoring damage and natural weathering damage. Rebuild stone at buttress wing walls as per original design and install new precast caps.</p>		421,455.00	421,455.00	
2	Concrete Foundation: excavate, form and pour new footers as required and new pad. Engineering design is needed for firm pricing submittal.		27,337.00	27,337.00	
3	To furnish 6 window glass panels matching the remaining glass pieces as close as possible.	6	230.00	1,380.00	
<p>Budget Pricing Includes: Equipment and labor, site bathroom facilities, electrical generator, water tank, crane, forklift, truck hauling and all transportation.</p> <p>Budget Pricing Excludes Prevailing Wage Rates and Certified payroll reporting, traffic control, flaggers, removal of existing concrete footers and foundation.</p>					

This is a budget based on the plans and specifications or an onsite visit directed by others. Pricing is valid for six months and after six months a review of the conditions should be performed and re- evaluated.

**Total**

\$450,172.00

Date	Estimate #
4/11/2022	4978

3060 Walnut St. Denver, Colorado 80205  
303-297-2004  
FAX 303-296-2840  
www.brsrestores.com

Name / Address					
Colorado Department Of Transportation 10601 10th Street Greeley, CO 80634		Terms		Project	
		Pricing is Good for 30 days		Road of Remembrance Memorials	
Item	Description	Qty	Rate	Total	
1	Road of Remembrance Memorials OPTION A Total dismantle and reassemble all stone pillars. Both Pillars - Prepare structures and pump out water. Erect scaffolding as needed to carefully remove all stone elements of each pillar. Categorize, palletize and transport all stone to the new location. Install salvaged stone as per original design. Replace the unsalvageable stone that is damaged during the demolition process with close match new stone to match existing as close as possible, install with the existing. Install new precast caps and install three windows at each pillar to match the original design.		689,479.00	689,479.00	
3	Concrete Foundation: excavate, form and pour new footers as required and new pad. Engineering design is needed for firm pricing submittal.		27,337.00	27,337.00	
4	To furnish 6 window glass panels matching the remaining glass pieces as close as possible.  Budget Pricing Includes: Equipment and labor, site bathroom facilities, electrical generator, water tank, forklift, truck hauling and all transportation Budget Pricing Excludes Prevailing Wage Rates and Certified payroll reporting, traffic control, flaggers, removal of existing concrete footers and foundation.	6	230.00	1,380.00	

This is a budget based on the plans and specifications or an onsite visit directed by others. Pricing is valid for six months and after six months a review of the conditions should be performed and re- evaluated.

**Total** \$718,196.00



April 28, 2022

## **PROPOSED ALTERNATIVES FOR THE FUTURE OF THE GATEWAY PILLARS**

For the past few months, a Mitigation Working Group composed of roughly a dozen civic, veterans, and historic preservation groups from across the Boulder County have engaged in a dialogue with our local community regarding the future of the Gateway pillars located on Arapahoe Road just west of its intersection with US 287. Scores of ideas have been discussed, investigated, and debated.

The Working Group now has narrowed the list to the following alternatives which appear to be potentially feasible and have some community support (in order of distance from current location):

1. Current Location
2. Proposed Intersection of Arapahoe Road and Aspen Ridge Drive
3. Forest Park
4. Cross Ridge Park
5. Legion Park
6. US 36 Overlook

A more detailed description of each of these alternatives is attached. Over the next few weeks, the Working Group will continue to seek community input concerning, and investigate the viability of, these alternatives. Members of the community are encouraged to offer their comments and suggestions concerning these alternatives through the portal at [9milecornerpillars.com](http://9milecornerpillars.com).

The goal of the Working Group is to finalize a list of one to three alternatives, and present them this summer for final public discussion.

## **The Gateway Pillars at the Current Location**

One alternative for the pillars' future is that they remain in or near their current location. Two approaches have been suggested:

1. *Leave the pillars where they presently sit, undertake necessary repairs and reinforcement of the structures, and add signage to identify the pillars to passing motorists.*

This approach reflects the view that though the setting of the pillars has been substantially eroded over the years, the pillars in their current location remain a significant East County landmark and must be preserved *in situ*. Relocation, according to this view, destroys “what makes the pillars the pillars.” This approach also would be less expensive than any relocation options.

The primary disadvantage of this approach is that it discounts the recent findings by CDOT that the 2021 widening project severely impacted the physical integrity and historic significance of the pillars, and added a new and serious vehicular hazard for eastbound traffic. To somewhat mitigate these concerns, the following steps could be undertaken:

- Repairs and additional buttressing of the pillars and their footings could reduce the impacts of the recent construction on their physical integrity.
- The “pork chop island” where the south pillar is located could be redesigned to remove utility fixtures, add landscaping and signage, and replace the guardrails with well-designed knee walls or other more attractive protective structures. Similar changes could be made in the vicinity of the north pillar to improve its visibility and appearance.
- Removing the steel guardrails and replacing them with attractive but carefully engineered protective structures could reduce the dangers to vehicular traffic.

The net result would be a mitigation, but not elimination, of the issues reflected in the recent CDOT survey. The pillars would remain enmeshed in a complex, busy intersection with all of the impacts noted by CDOT. They are likely to be further impacted by additional construction at the intersection within the next two decades. Indeed, a Traffic Impact Study commissioned in November 2019 by the developer of Nine Mile Corner concluded that an additional left turn lane at that intersection will be required on eastbound Arapahoe Road by 2040, necessitating a further widening of that road at the current location of the pillars. Nonetheless, if keeping the pillars in their current location is deemed to be of primary importance, then these steps could moderate some of the impacts of the recent construction.

2. *Same as prior approach, but also reconfigure the intersection in some unspecified way to better protect and display the pillars.*

Recognizing that the prior option does not resolve the serious impacts of the 1983 and 2021 widening projects on the Gateway's historic setting, it has been suggested that the intersection be reconfigured to somehow restore the original appearance of the pillars and the surrounding area. Multiple, additional objections to the current intersection design – many of which have nothing to do with the pillars – also have been raised.

Unfortunately, after lengthy discussions, no alternative design has been proposed that is compatible with the pillars' historic setting, while at the same time providing a safe and efficient

intersection for current and anticipated traffic flows. Even if one could design the intersection on a blank sheet of paper, with the only requirements being that (a) the pillars be kept in or near their original locations, and (b) the intersection safely and efficiently handle the anticipated volume of traffic, it seems unlikely that a viable alternative configuration could be developed. Nonetheless, parties are encouraged to propose any designs that meet these criteria.

## **Relocation of the Gateway Pillars to the Intersection of Arapahoe Road and Aspen Ridge Drive**

A major challenge in finding a solution to the current situation with the Gateway pillars is developing plan that (a) maintains some semblance of their original setting, (b) is feasible, and (c) provides for their future. The following are two proposals that might meet these goals.

### **Background**

Many community members have commented that the original location of the pillars – which was preserved for nearly a century – both marked the symbolic entrance to the City of Boulder and formed a visual gateway to Arapahoe Road framed by open farmland and the mountain backdrop to the west. Much of that character has been lost with the two intersection widening projects in 1983 and 2021. Arapahoe Road west of the current location, however, retains some of its original 1928 character. That is, it is a narrow two-lane road predominantly flanked by open land, which still offers sweeping mountain vistas to the west. Moving the pillars a few hundred feet to the west could recapture much of their original character.

But the nature of that portion of Arapahoe Road will soon change. The City of Lafayette has approved construction of a development currently known as the Silo subdivision on the south side of Arapahoe beginning about a quarter mile west of the pillars' current location. Another commercial development is being planned by the Tebo Partnership, LLP for the land on the south side of Arapahoe between the pillars and the Silo site.

To accommodate those two developments, a new arterial – to be known as Aspen Ridge Drive – is planned for the south side of Arapahoe Road along the boundary between the properties. Current plans call for installation of a new traffic light at the Arapahoe Road – Aspen Ridge Drive intersection, roughly ¼ mile west of the Arapahoe Road – US 287 intersection.

Moreover, it also appears probable that traffic flows soon will require that Arapahoe Road be widened. A Traffic Impact Study commissioned in November 2019 by the Nine Mile Corner developer predicts that traffic on Arapahoe between US 287 and 95<sup>th</sup> Street will increase by 35% 2021 and 2040. Such increased traffic flows presumably will increase the pressure to widen that portion of Arapahoe Road to four lanes.

At the same time, both Boulder County and the City of Lafayette are taking steps to preserve the semi-rural character of many of the properties along Arapahoe. Even the planned developments, though obviously changing the appearance of those tracts, include setbacks, open space, and other features adjacent to Arapahoe Road that will temper the urban feel of the area. As a result, Arapahoe Road will retain some of its rural character and most of its mountain views for westbound traffic coming from the US 287 intersection.

Because the reconstruction of Arapahoe Road in the vicinity the new developments has not yet begun, there may still be time to influence the design of the Arapahoe Road – Aspen Ridge Drive intersection to incorporate a new home for the Gateway pillars. Below is a proposal for such a redesign, which recreates some semblance of the pillars' original setting in a location unlikely to be impacted by additional future changes to Arapahoe Road. In addition, a less ambitious option for placing the pillars in a park near the intersection is discussed.

## **Proposal A – A Traffic Circle at the Intersection of Arapahoe Road and Aspen Ridge Drive**

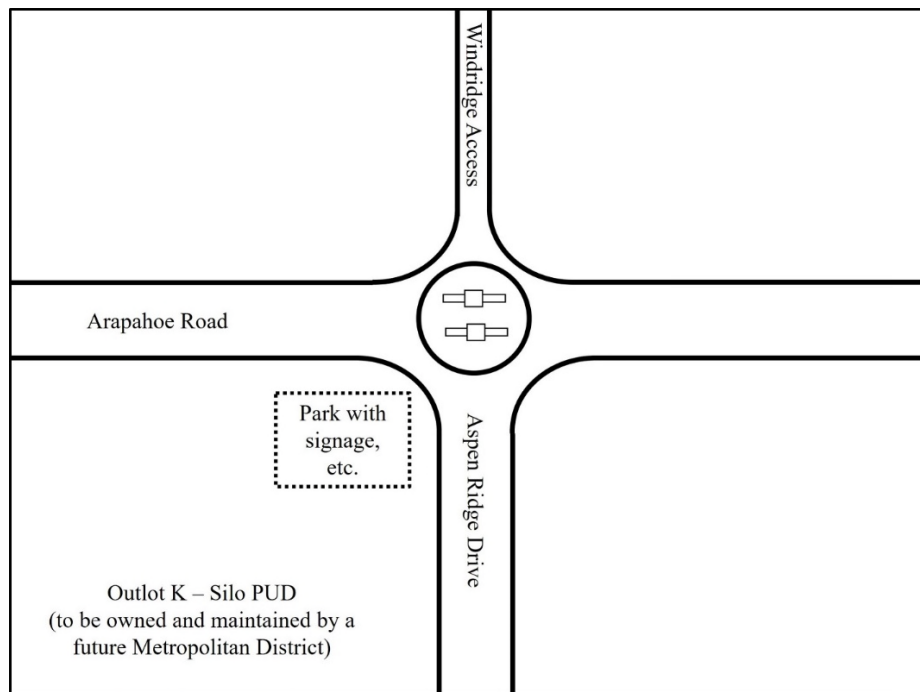
The primary stumbling block to recreating a new “gateway” using the pillars on Arapahoe Road is the likelihood that the highway will be widened so that (a) the pillars must again be moved, and/or (b) their gateway character will be lost. One answer to that obstacle is to route Arapahoe Road around the pillars. In that fashion, if Arapahoe Road is again widened in the future, the new width could be added to the outer lanes without impacting the pillars.

This design could be accomplished by constructing a traffic circle at the intersection of Arapahoe Road and Aspen Ridge Drive, with the pillars relocated in the middle of the traffic circle. Simple signage identifying the pillars for drivers could be placed on the circle, with perhaps some landscaping, lights, or other features.



*Exemplar traffic circle – Erie Parkway and 119<sup>th</sup> St.*

While the traffic circle would not be readily accessible by foot, a small park could be constructed on the SW corner of the intersection with more detailed interpretive signage, historic photos, etc. This park could be accessed on foot from either the Silo or Tebo developments.



*Concept for Relocation to Arapahoe Road – Aspen Ridge Drive Intersection*

## *Advantages*

The proposed design, if adopted, would have several advantages:

- Those in vehicles approaching the traffic circle on Arapahoe Road would see the pillars directly ahead, not off to the side. While vehicles would not pass between the pillars, the view from the east and west would preserve and highlight their historic gateway character.
- Approaching from the east, the pillars' backdrop would be open land with mountains in the distance, approximating the view down a Road of Remembrance planned in 1928.
- The pillars will be enjoyed by thousands of motorists each day since they are squarely in view, not relegated to a side venue where special effort is required to see them.
- Passing motorists primarily will be the same Boulder County residents who currently enjoy the pillars, many with an attachment and appreciation for their history.
- The adjacent park would provide space and opportunity to erect signage to explain the pillars' history and significance. If appropriate, the park could be used to expand the memorial purpose of the pillars to honor veterans of other wars.
- Compared to a traffic signal, a traffic circle would enhance the appearance of Arapahoe Road and the entrance to the two developments.
- The current status of the intersection of Arapahoe Road and Aspen Ridge Drive presents an opportunity to design and build such a configuration from scratch, rather than retrofitting or squeezing into an existing site.
- By including the traffic circle in the initial construction of the intersection, the expense of some of the sitework could be included in the cost of the highway improvements.
- The pillars and park could potentially be owned by a governmental entity, thus providing for perpetual care and maintenance.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

## *Disadvantages*

As one might expect, there also are obstacles inherent in the proposed design:

- The project is ambitious, requiring the consent and cooperation of multiple parties.
- The timeline for completing the new Arapahoe Road – Aspen Ridge Drive intersection is uncertain, but may be several years away.



- The new Arapahoe Road – Aspen Ridge Drive intersection will be located at the top of a slope on a highway currently with a 50-mph speed limit. There may be concerns about placing a traffic circle in such a location. On the other hand, those same issues will arise regardless of what design is adopted for the new intersection. The expertise of traffic engineers will be needed to determine the nature and extent of any additional danger posed by a traffic circle.

- There may be concerns about whether a traffic circle would interfere with traffic flows on an already congested Arapahoe Road. On the other hand, a traffic circle might improve the efficiency of traffic movement compared to the currently planned signal. These relative efficiencies should be addressed by traffic engineers to determine the feasibility of the proposal.

- No pedestrian access to the pillars will be provided. However, the original Gateway was designed to be viewed from passing vehicles; even the signage (plaque, cornerstone) was designed to be seen from a passing vehicle. This design retains that concept, plus the adjacent park will have pedestrian access, where explanatory signage or memorials can be placed.

### ***Discussion***

Though multiple details of this plan need addressing, some are discussed below:

*Land acquisition for the traffic circle:* Ownership of the land needed for a traffic circle, put mildly, is a complex issue:



*Land Ownership at Proposed Arapahoe Road – Aspen Ridge Drive Intersection*

A traffic circle would be built principally on the right of way for Arapahoe Road (CO 7), which is owned by CDOT. But regardless whether the pillars are relocated to the Arapahoe Road – Aspen Ridge Drive intersection, it appears probable that some additional right of way will be required for that intersection and/or a widened Arapahoe Road. If the proposed traffic circle can be constructed within the existing and already planned right of way, then no additional land will be required.

Until the final design of the intersection is completed, it is impossible to know if additional land will be required. Nonetheless, while it appears that much of the traffic circle and surrounding traffic lanes can be accommodated in the existing right of way, the geometry of the proposed traffic circle and surrounding traffic lanes probably will require that small amounts of additional right of way be acquired from each of the four landowners bordering the new intersection (see map above).

The southwest corner. The land on the southwest corner of the new intersection is currently owned by Hearteye Village Land Company, which in 2017 entered into a PUD for the Silo development with the City of Lafayette. That PUD includes a plan for the construction of Aspen Ridge Drive along the eastern boundary of the Silo subdivision where it will intersect with Arapahoe Road.

In addition, that PUD designated Outlot K on the northeast corner of the Hearteye property – some of which might be needed for a traffic circle – to be used as a park. According to the PUD, this park will be owned and maintained by the Weems Neighborhood Metropolitan District (WNMD), a quasi-governmental entity encompassing the Silo subdivision (see above diagram). However, Hearteye has not yet transferred Outlot K to WNMD.

Hearteye later agreed to sell the Silo subdivision to Cornerstone Homes Development Company. Hearteye and Cornerstone subsequently entered into a Development Agreement with the City of Lafayette on May 2, 2019. That Agreement provides that the Silo project would be developed in two distinct phases, with each phase occurring over an approximately ten-year buildout period. Phase 1 would develop the southern portion of the property, leaving the northern portion abutting Arapahoe Road for Phase 2.

In addition, the Development Agreement made specific reference (§27b) to the installation of a traffic signal at the Arapahoe Road – Aspen Ridge Drive intersection. That Agreement provided that Cornerstone would install a traffic signal at that intersection in accordance with CDOT specifications. The precise timing of the construction of the new intersection is not stated, since it depends on other factors specified in that Agreement.

The Agreement further provided that Cornerstone would be responsible for 50% of the cost of the traffic signal, while the owner of the adjacent Tebo Partnership property should be responsible for the remaining 50%. However, if CDOT required construction of the traffic signal before the contribution by the owner of the Tebo Partnership property could be secured, Cornerstone would be responsible for 100% of cost, subject to a right of later reimbursement. The Development Agreement also contains a number of other specific provisions related to the development of the land in Phase 2 adjacent to Arapahoe Road.

On May 3, 2019, Cornerstone sold the land for Phase 1 to Lennar Colorado, LLC, and assigned its rights and obligations under the Development Agreement to Lennar. At the same time, the various parklands, open space, and other land designated on the PUD for WNMD in Phase 1 – but not Phase 2 – were transferred to WNMD.

Accordingly, at the present time, it appears that:

- Hearteye remains the legal owner of the land for Phase 2, including Outlot K on the northeast corner of the Silo development. However, per the PUD, when Phase 2 is developed, Hearteye is obligated to transfer Outlot K to WNMD. With the agreement of the parties, any land needed for the traffic circle and/or the memorial park could be set aside prior to that transfer.
- Lennar, as assignee of the Development Agreement, is obliged to construct the traffic signal at the Arapahoe Road – Aspen Ridge Drive intersection. Since work on that intersection has yet begun, funding earmarked for the signaled intersection could be used to construct a traffic circle.

The southeast corner. The Silo PUD and other City of Lafayette planning documents anticipate that the Arapahoe Road – Aspen Ridge Drive intersection will be a primary access for any development on the Tebo Partnership LLP property. The Development Agreement for the Silo subdivision accordingly anticipates that the Tebo Partnership LLP or its successor will contribute 50% of the cost of the intersection improvements. However, it appears that no agreement has been reached between the City of Lafayette and the Tebo Partnership LLP concerning those matters. Again, if the parties agreed, funds required to construct the signaled intersection could be repurposed to construct a traffic circle.

The northeast corner. The land on the northeast corner of the new Arapahoe Road – Aspen Ridge Drive intersection is owned by Stephen Tebo, an owner of Tebo Partnership LLP which owns the property directly south across Arapahoe Road. Though the plans for the northern parcel are not known, it seems possible that an arrangement might be negotiated with Mr. Tebo to acquire a small amount of additional land for the traffic circle from that parcel, if needed.

The northwest corner. The parcel adjacent to the northwest corner of the new intersection includes a substantial residence and agricultural operation. The driveway for that property is directly across Arapahoe Road from the planned entrance and traffic signal at Aspen Ridge Drive (see above diagram). The property was owned by Richard P. Miller, who recently passed away and it appears that the property is currently controlled by his estate while his affairs are being wound up. Though any discussions concerning the acquisition of land for the proposed traffic circle could be complicated by the estate proceedings, Mr. Miller's estate also has some incentive to have the intersection plans finalized prior to any transfer of the Miller property.

Moreover, the Miller property is subject to what was known as the Windridge PUD, which required the landowner to execute a conservation easement in favor of Boulder County on the land adjacent to Arapahoe Road. While the County cannot unilaterally transfer a portion of that land for intersection improvements, its easement rights lessen the value of the land for the Miller estate.

Conclusion: Assembling the land for a traffic circle would be a complicated, time-consuming process. On the other hand, that amount of land at issue for each landowner should not be significant, and would include land already significantly impacted by the planned signaled intersection. Further, the adjacent landowners may embrace the new design, since replacing a traffic signal with a traffic circle could benefit their properties.

*Land acquisition for the Park:* The proposed park on the SW corner of the intersection would not be located in the CDOT right of way, but would be developed on a portion of Outlot K in the Silo subdivision. Since the PUD designates Outlot K as a small park to be owned and maintained by WNMD, the placement of memorial signage or other similar items relating to the Gateway pillars in that park would seem to be compatible with those plans.

*Design:* If the traffic circle proposal is pursued, community input should be solicited regarding the various design elements. One appealing design envisions the pillars situated on an east-west axis and standing parallel, similar to their original configuration, but only a few feet apart. A paved, brick, or stone path could run between them on the traffic circle, representing the Road of Remembrance. Signs on the traffic circle could identify the pillars and their memorial purpose. If appropriate, the originally planned external and internal lighting could be reworked to highlight the pillars at night.

Careful engineering will be needed to provide footings, structural reinforcement, and other design features to mitigate any damage from the vibrations from passing vehicles. Similarly, a knee wall or other low, unobtrusive barrier – similar to those used on many other traffic islands (see photo, above) – may be needed on the circumference of the traffic island to provide protection from errant drivers. Since the pillars presumably would be erected on slightly elevated footings for drainage purposes, such a wall should not obstruct the view from passing vehicles.

There are countless possibilities for the adjacent memorial park. Various signs, historical photos, plaques, or memorials could be installed. Like the original Gateway park, the new park could have a flagpole and perhaps even a replica canon. Depending on community sentiment, the park could honor only those who served in WWI or a broader segment of the veteran community.

*Ownership and future maintenance:* Under this proposal, the Gateway pillars would be located on a traffic circle on a CDOT-owned right of way, and the adjacent memorial park would be on property owned by WNMD. Both the City of Lafayette and Boulder County also have interests in the Gateway pillars and the area.

Logically, the maintenance obligations for both the pillars and the adjacent park should be consolidated under one governmental entity. While WNMD is a possibility, the precise contours of such an arrangement will need to be negotiated between the relevant authorities and memorialized in an intergovernmental agreement or similar instrument.

### **Proposal B – Memorial Park at the Intersection of Arapahoe Road and Aspen Ridge Drive**

A less ambitious alternative would be to relocate the pillars to the planned park on Outlot K of the Silo subdivision on the southwest corner of this intersection (see diagram, above). As previously discussed, that land is currently owned by Hearteye but, under the terms of the Silo PUD, Outlot K is slated to be transferred in Phase 2 to WNMD for use as a park. Particularly since

that area is still in the design stage, it seems feasible to discuss relocating the Gateway pillars to the planned park.

### ***Advantages***

This alternative, if adopted, would have several advantages:

- The pillars would be visible to thousands of vehicles approaching the intersection on westbound Arapahoe Road, roughly ¼ mile from their original location.
- Passing motorists primarily will be the same Boulder County residents who currently enjoy the pillars, many with an attachment and appreciation for their history.
- The park should provide space and opportunity to erect signage to explain the pillars' history and significance, and pedestrian access to the pillars and the amenities. If appropriate, the park could be used to expand the memorial purpose of the pillars to honor veterans of other wars.
- The current status of the intersection of Arapahoe Road and Aspen Ridge Drive presents an opportunity to design and build such a park from scratch, rather than retrofitting or squeezing into an existing site.
- By including the park in the initial construction of Phase 2 of the development, the expense of some of the sitework could be included.
- The pillars and park could potentially be owned by a governmental entity, thus providing for perpetual care and maintenance.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### ***Disadvantages***

There also are obstacles inherent in the proposed design:

- The pillars will be used solely as a monument, losing their unique gateway character and with less visibility than the present location for westbound traffic on Arapahoe Road.
- The pillars will have little visibility for eastbound traffic on Arapahoe Road.
- The timeline for completing Phase 2 of the Silo development is uncertain, but may be several years away.
- Outlot K is traversed SW to NE by the Davidson-Highline Lateral Ditch, as well as planned stormwater and other easements. While smaller structures such as signage or benches likely should not interfere with those uses, significant engineering and planning would be required

to place structures the size of the pillars on that tract. A preliminary review suggests that the pillars would need to be relocated some distance from Arapahoe Road, decreasing their visibility from passing vehicles.

- Though Phase 2 has not been platted, under the design reflected in the PUD does not include an public parking in the vicinity of Outlot K, potentially raising conflicts with the residents of the development.

### **Conclusion**

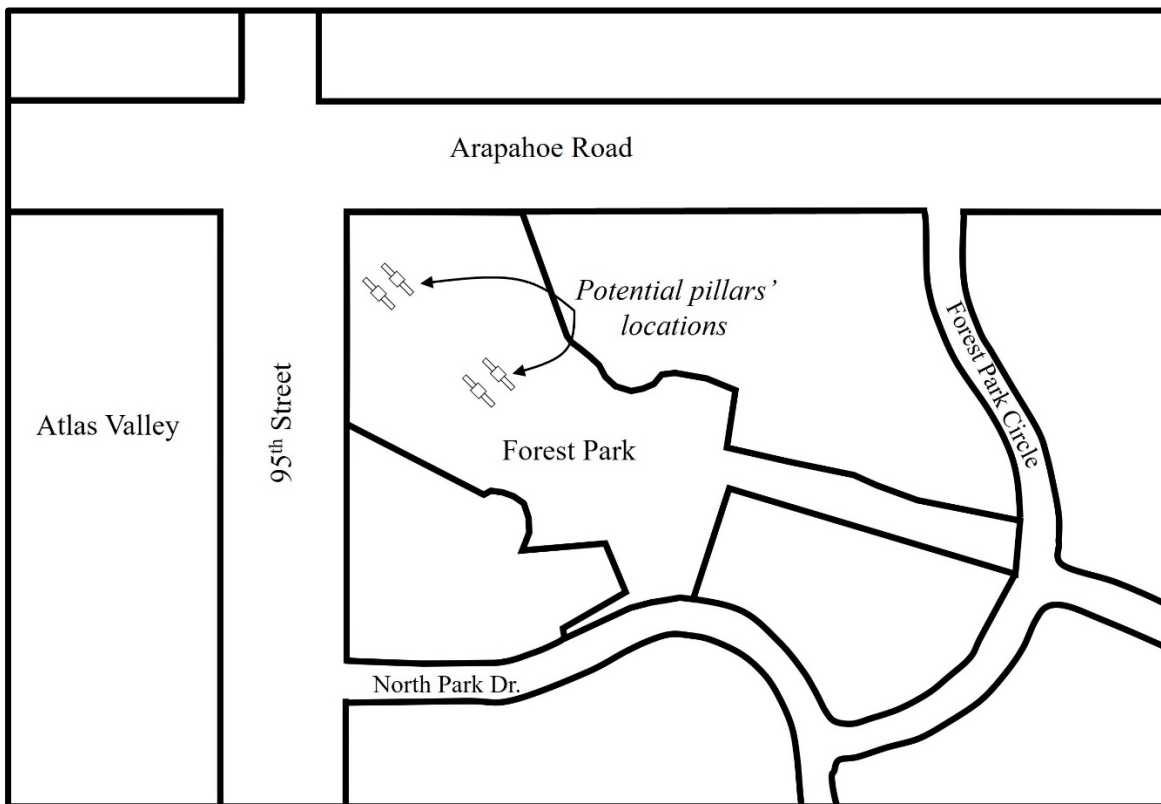
Decades of neglect have allowed the purpose and setting of the Gateway pillars – a community memorial to those who served in WWI – to be lost in the sprint for development. Proposal A cannot return the clock to 1928, but may offer a path for reclaiming at least part of the community’s vision and for honoring those who sacrificed during that horrible conflict. If that idea is not implemented, Proposal B may offer the opportunity to relocate the pillars to a park close to their original location.

## Relocation of the Gateway Pillars to Forest Park

Recent community comments outlined a consensus that a future home for the Gateway pillars should be to a site (a) where they will not again be subject to further movement or disturbance, (b) which is governmentally owned, (c) where efforts can be made to enhance the pillars' visibility as a memorial, and (d) where both pillars can be located. Community members also have expressed a preference that the pillars be relocated to a site as near as possible to the current location, and with reasonable visibility for persons in vehicles passing by on a nearby highway or street. Relocation of the pillars to Forest Park at the southwest corner of Arapahoe Road and 95<sup>th</sup> Street generally meets these criteria.

### Background

Forest Park is currently owned by the City of Lafayette. The parcel is slightly more than 4 acres in size, and was created as Outlot F when the Forest Park subdivision was approved in 1998.



*Forest Park Location Map*

Though the park is owned by the City of Lafayette, it is maintained by the Forest Park Village homeowner's association. The landscaping is mature with grass open spaces and numerous mature trees. The area along the north boundary of the park adjacent to Arapahoe Road is sunken below the grade of the adjacent roadway to serve as a catch basin for storm runoff.





*Forest Park from the southeast corner of Arapahoe Road and 95<sup>th</sup> Street*

### **The Proposal**

The location and ownership of Forest Park make it a viable site for relocating the Gateway pillars as part of a memorial to soldiers who served in WWI, and potentially including those who served in other conflicts. While the relocated pillars likely would not be easily seen on Arapahoe Road, the park is within the general vicinity of the pillars' original location and provides ample space for creating a memorial featuring the pillars along with signage, landscaping, and other features.

### **Advantages**

The proposed design, if adopted, would have several advantages:

- The park is situated on Arapahoe Road, the planned "Road of Remembrance," roughly 1½ miles from the pillars' original location.
- The park's location is most visible to eastbound traffic on Arapahoe Road and southbound traffic on 95<sup>th</sup> Street. Depending on the location of the pillars within the park, they may or may not be visible to this traffic.



- The park would provide ample space to create a memorial park among the mature trees in a pleasant setting, with the pillars as a centerpiece accompanied by explanatory signage and other amenities.
- The nearby parking lot for Forest Park Village provides public parking for those who want to visit the pillars.
- Compared to other alternatives, this site may be less expensive since the park is accessible with some infrastructure in place.
- Since the park is owned by the City of Lafayette, the process of reaching an agreement may be less complicated.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

There also are obstacles inherent in the proposal:

- Regardless where the pillars are sited in the park:
  - They will lose their unique gateway character, and
  - They will have little visibility for westbound traffic on Arapahoe Road.
- If the pillars are relocated to the interior of the park away from the northwest corner to create a memorial park, their visibility from the roadways is essentially eliminated.
- If the pillars are relocated to Forest Park, mature landscaping will need to be removed. The City and homeowner's association likely would not be supportive of removing existing mature trees, and would require replacing them with trees of a matching caliper. The City probably also would require a formal landscape plan, prepared by a landscape architect, that would include a new irrigation plan to accommodate those changes.
- The commercial interests in Forest Park Village may object to (or may like) the possibility that the memorial park could bring additional visitors to park in the commercial spaces.
- The current arrangement for provides for the Forest Park Village homeowner's association to maintain the park. If the pillars are moved to Forest Park, some modification of that arrangement may be required, and the City would need to agree to be responsible for the perpetual maintenance of the pillars and any amenities.

## **Discussion**

If the decision is made to create a memorial park featuring the Gateway pillars in the general vicinity of the current location, then Forest Park is a viable option. Since the property is owned by the City of Lafayette, discussions for creating such a park could involve fewer parties and less legal complication. Moreover, though the presence of mature trees and landscaping may pose obstacles, they also could provide a peaceful setting for the memorial.

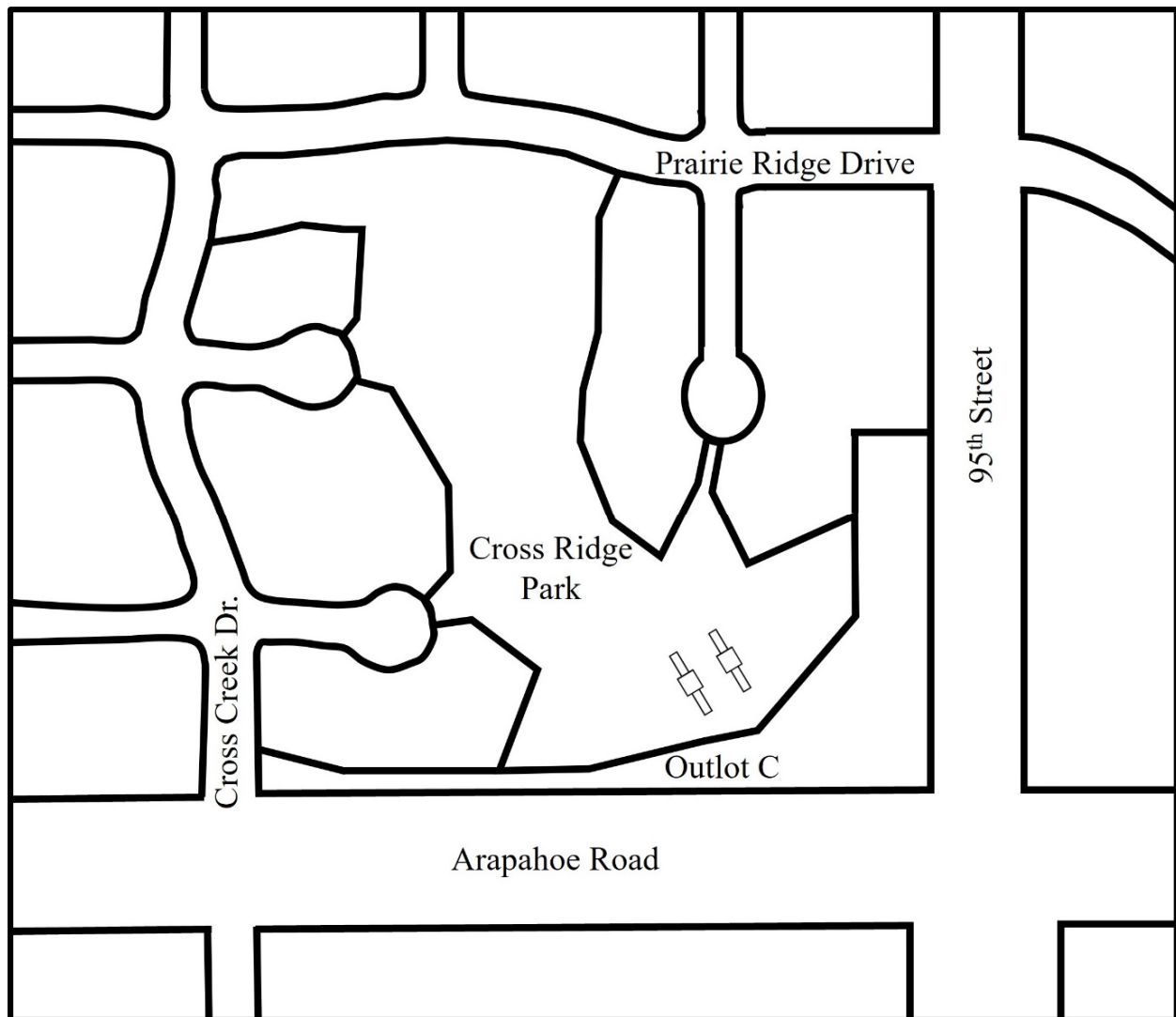
The primary decision for the community is whether to substantially change the nature of the pillars' setting, and use them as the centerpiece for a pedestrian-accessed memorial park rather than as a monument designed to be primarily viewed from a vehicle. If the former is selected, then Forest Park may be a good choice.

## Relocation of the Gateway Pillars to Cross Ridge Park

Like Forest Park and some other alternative locations, Cross Ridge Park could serve as a site for relocating the pillars. The site (a) likely will not be impacted by future road expansions, and thus would avoid further movement or disturbance of the pillars, (b) is governmentally owned, (c) has space to add amenities to enhance the pillars' use as a memorial, and (d) can accommodate both pillars. Cross Ridge Park also is relatively near the current location, and with some visibility for persons in passing vehicles.

### Background

Cross Ridge Park is a 4.74 acre neighborhood park managed by the City of Lafayette. The park was created in 1994 as part of the Cross Creek subdivision.



*Concept for Relocation to Cross Ridge Park*

The bulk of the park is an open, grassy area. The portion nearest to the corner of Arapahoe Road and 95<sup>th</sup> Street is significantly below the grade of the adjacent roadways, and serves as a catch basin for storm runoff.



*Cross Ridge Park – view from NW towards Arapahoe Road – 95<sup>th</sup> St. Intersection*

Between the park and the roadways on the west and south is a strip of land designated as Tract C in the Cross Creek subdivision filing, which is owned by the Ridge at Cross Creek Homeowners Association. The portion of this strip on the corner of Arapahoe Road and 95<sup>th</sup> Street is landscaped with mature trees and contains part of a path that circumnavigates the park.

### **The Proposal**

The location, orientation, and ownership of Cross Ridge Park make it a viable site for relocating the Gateway pillars. While there are a number of possible locations for pillars in the park, one site would be at the southeast end of the park west of the mature trees and as close as feasible to the northwest corner of Arapahoe Road and 95<sup>th</sup> Street. In that location, the pillars could be oriented on a somewhat NW-SE axis so that the “gateway” between them is visible to westbound traffic. In addition to the pillars, there is ample room in that area of the park for signage, landscaping, and other features that would be somewhat visible to westbound traffic and accessible via the existing walking path in the park.

### **Advantages**

The proposed design, if adopted, would have several advantages:

- The park is situated on Arapahoe Road, the planned “Road of Remembrance,” roughly 1½ miles from the pillars’ original location.
- The park’s location on the northwest corner of the Arapahoe Road – 95<sup>th</sup> Street intersection is somewhat visible to westbound traffic on Arapahoe Road, the original intended audience for the Gateway pillars. Depending on the location, the pillars could also be somewhat visible to north and/or southbound traffic on 95<sup>th</sup> Street.
- The motorists viewing the pillars primarily will be the same Boulder County residents who currently enjoy them.
- The park would provide ample space and opportunity to erect signage to explain the pillars’ history and significance. If appropriate, the park could be used to expand the memorial purpose of the pillars to honor veterans of other wars.
- The combined park and outlot contain open spaces and a walking path, reducing the amount of work necessary to prepare the site.
- If sited in the existing park, the pillars and park would be owned by the City of Lafayette, which would agree to be responsible for perpetual care and maintenance.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

The obstacles inherent in the proposal are as follows:

- If the pillars are relocated into the existing park, they would:
  - Be some distance from and substantially below the grade of Arapahoe Road, reducing their visibility to passing motorists, and
  - Lose their unique gateway character.
- Regardless of the siting, the pillars will have poor to non-existent visibility for eastbound traffic on Arapahoe Road.
- The siting of the pillars is complicated by the fact that it is a detention pond converted into a park. The park receives all the storm drainage for that neighborhood, so that the southeast corner of the park holds water during many weather events and even during snowmelt.

The flat area of the park has an engineered and installed drainage system. Any changes to that area likely would require that both the drainage and irrigation systems be re-engineered and rebuilt.

- To relocate the pillars close to the corner of the intersection, some agreement would be required to transfer a portion of Outlot C to the City of Lafayette.
- The siting of the pillars is complicated by the presence of mature trees on Outlot C close to the corner of the intersection. If the pillars are sited behind the mature trees, or west of the corner to avoid the trees, the pillars' visibility from the roadways will be further reduced.
- Cross Ridge Park was designed as a neighborhood park, with little adjacent parking. Residents in the Cross Creek subdivision should be consulted regarding the addition of the pillars and associated amenities to that park, and likelihood of additional traffic in the neighborhood.

### **Discussion**

In comparison to some alternatives, relocating the Gateway pillars to Cross Ridge Park would involve fewer parties and less legal complication. If the City of Lafayette agreed to the proposal, the legal requirements and approvals would be significantly reduced, and the project simplified. Those issues could be more difficult if the pillars were moved partly or entirely on to a portion of Outlot C currently owned by the Cross Creek Homeowners Association.

Though the park is relatively open land, the site poses significant engineering complications given its use as a stormwater catch basin. The presence of mature trees on the corner, and the tract's use as a neighborhood park could inject additional issues.

Finally, relocating the pillars into a park, even if visible from a roadway, changes their character and the public's perception of them. While their history could be explained – likely better than it is explained now – the pillars would become more of a static memorial than part of the everyday fabric of the East County.

## **Relocation of the Gateway Pillars to Legion Park**

Of all of the potential alternative sites for the pillars, Legion Park aligns most closely with the original memorial purpose of the pillars. Dedicated in 1934, Legion Park was created as “a memorial to the soldier dead of the World War.” Though never formally associated with the Road of Remembrance, the park accordingly shares the same memorial purpose as the Gateway pillars. Moreover, Legion Park meets other criteria identified by the community including that it is a site (a) where the pillars will not again be subject to further movement or disturbance, (b) that is governmentally owned, (c) where efforts can be made to enhance the pillars’ visibility as a memorial, and (d) where both pillars can be located.

### **Background**

In 1917, a local Boulder businessman, John Howard Empson, deeded 28 acres to Boulder County, including Goodview Hill on the north side of Arapahoe Road overlooking the Boulder Valley. The parcel was later reduced in size to ~23 acres when the County dedicated a strip along the south boundary for the newly build Arapahoe Road.

In 1931, Boulder County commissioners requested the construction of a new scenic road to the top of Goodview Hill to provide a vantage point from which visitors will get an “unparalleled panoramic view.” When this opportunity arose, Post 10 in 1932 leased the Goodview Hill property from the County “for the purpose of beautifying said tract as a memorial to the soldier dead of the World War.” The lease, which bears a rent of 50¢ per year, expires on October 31, 2031.

Thereafter, the memorial was designed by Saco Rienk DeBoer, a noted landscape architect from Denver. His design for the memorial called for 2’ high rock wall, 382 feet long, and 100 feet wide with flagpoles located at each end, located at the top of Goodview Hill. Two captured artillery pieces (from the same source as the cannon at the Gateway park) were installed on platforms near the flagpoles. Two elaborate entrance gateways were planned for the entrance road off Arapahoe Road.

A crew from the Civilian Conservation Corps constructed the hilltop memorial, but the entrance gateways were never built. Legion Park was dedicated in late June 1934, and maintenance responsibilities were turned over to Legion Post 10. However, a later study concluded: “It is unclear what work, if any, occurred on the property after the American Legion signed the lease since the country was fast approaching the lowest point of the Great Depression.”

Over the years, the park fell into significant disrepair. Legion Post 10 told the *Daily Camera* that they tried for years to maintain the park, but the vandalism remained rampant. Visitors stole the flagpoles and plaques. The Legion provided garbage cans, but they were stolen. The legion Commander was quoted as saying “we put out trash bags, but some of the bums slept in them.”

In early 1976, due to the County’s concern of the ability of Post 10 to properly maintain the park, Boulder County Parks and Open Space Department (BCPOS) took control of the park’s maintenance, though the Legion continued its sponsorship of the memorial. Toward that end, the County signed a Memorandum of Agreement with the Legion confirming their March 1, 1933 lease, but providing that the County would take responsibility for the care of the park. In this same

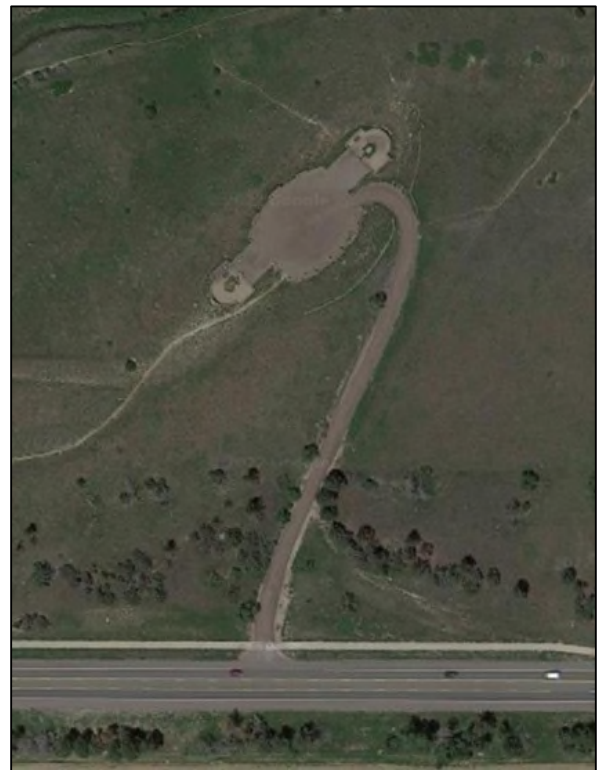


MOA, it was agreed that the Legion would maintain all war memorials, flag poles, plaques, and signs, continue to conduct memorial services, erect fitting displays in memory of the war dead, install additional war memorial monuments at its expense. The Legion also was given the right to formally name the park.

On Memorial Day, May 30, 1977, the Legion rededicated the park as “Boulder Veterans Memorial Park.” However, the park continued to decline. One of two artillery pieces disappeared, apparently in the 1970s. In June 2005, the Legion arranged for the Colorado Air National Guard to remove the other artillery piece, which the Legion restored and now displays at its building on North Broadway in Boulder.

Though officially named “Boulder Veterans Memorial Park,” the site is now referred to Legion Park, and Goodview Hill is now called “Legion Hill,” by the County and on most maps. However, since the last artillery piece was removed in 2005, it does not appear that Legion Post 10 has conducted any activity or done any further maintenance or improvement to the site.

Instead, County publications list Legion Park as one of its “open space properties” managed by BCPOS. The memorial at the crest of Goodview Hill been reduced to the perimeter rock wall; all of the other memorial features (artillery pieces, flagpoles, plaques) have been removed. The interior space of the memorial now serves as a parking lot for those seeking to enjoy the view, with picnic benches, two Parks and Open Space interpretive signs, and trash and recycling containers maintained by the County.



*Legion Park*

The remainder of the 23-acre property is dryland, with a 0.9 mile loop trail used for hiking, mountain biking, and other recreational purposes. It is maintained as non-irrigated open space with native or invasive prairie vegetation. Apart from the access road from Arapahoe, a “Legion Park” sign, and the loop trail, the remaining property is vacant.

On October 8, 2019, the Board of County Commissioners approved a request from BCPOS to designate Legion Park as a Boulder County Historic Landmark. This designation covers the entire 23 acres of the park, and provides that “Any construction or alteration of the landscape or features within the site area will require review and approval of a Certificate of Appropriateness.”



## **The Proposal**

During the community discussions concerning the future of the Gateway pillars, there has been significant discussion about relocating them to Legion Park. To date, however, there has been no consensus concerning the exact siting of the relocated pillars within the park. Among the locations that have been discussed are:

1. Flanking the entry drive at the bottom, set back a short distance from Arapahoe Road.
2. Flanking the entry drive about 100' – 150' up the hillside from Arapahoe Road.
3. Flanking the entry drive at the top of the hill at the entry to the memorial.



*Entry to Legion Park from Arapahoe Road*

4. On top of the hill, on the west side of the memorial's wall, adjacent to (or as part of) the existing wall.
5. On top of the hill, on the southwest corner of the memorial's wall, close to an existing wall, potentially moving up the hill.



*Current Memorial at Legion Park*

Depending on which location is selected, benches, interpretive signage, and other amenities could be installed.

### **Advantages**

The proposed location, if adopted, would have several advantages:

- Legion Park was created at roughly the same time as the Gateway by some of the same individuals and organizations that created the pillars, and shares the same memorial purpose with the pillars.
- Legion Park is situated on Arapahoe Road, the planned “Road of Remembrance,” albeit 4½ miles from the pillars’ original location.
- If sited to flank the entry road, the pillars would be visible to traffic on Arapahoe Road and continue to serve as a “gateway.”
- If sited on top of the hill, the area would provide ample space for explanatory signage and other amenities.
- The existing space at the top of the hill provides public parking for those who want to visit the pillars.
- Boulder Legion Post #10 has indicated a willingness to work with the Mitigation Working Group if the decision is made to relocate the Gateway pillars to Legion Park. Since the park is owned by Boulder County, if an agreement can be reached, the County could assume responsibility for the perpetual care and maintenance of the pillars and the associated signs and memorials.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

There also are obstacles inherent in the proposal:

- Legion Park is situated several miles from the pillars’ original location, where they were an integral part of daily life in the East County.
- Legion Park is scarcely known in Boulder County, and predominantly is used by residents of the City of Boulder. While moving the pillars to Legion Park may increase the park’s use and visibility, the pillars will be seen far less at that location than at their current or some other relocation sites.
- The park has a history of vandalism, and increased security and/or maintenance may be required.

- If the pillars are sited to flank the entry road:
  - A significant amount of earthmoving and site preparation would be required due to the slope; and
  - Explanatory signage and other amenities likely would need to be located at some distance from the pillars.
- If the pillars are sited on top of the hill, they would not be visible from Arapahoe Road and would not serve as a gateway.
- The entirety of Legion Park is designated as both a Boulder County Historic Landmark and as Boulder County open space.
  - The landmark designation is particularly important with respect to the memorial, which is protected both as a representative project constructed by the Civilian Conservation Corps and as a representative work of master landscape architect Saco Rienk DeBoer. The incentive to protect the historic integrity of the Legion Park memorial is significant, since that iconic landmark should not be degraded in the same way as the setting for the Gateway has been destroyed.
  - Boulder residents' well-known fierce protection of open space suggests that any relocation, particularly one that requires sitework which disturbs open space land, may encounter community resistance.
- If the Gateway pillars are relocated to Legion Park, they will lose their independent identity and simply become another structure in the park.

### **Discussion**

Legion Park in many respects is an attractive future home for the Gateway pillars. The park's temporal, architectural, and commemorative symmetry with the Road of Remembrance's Gateway is obvious. But the two memorials were created to be, and have been maintained for nearly a century as, independent sites. One was an impressive destination shrine, while the other was specifically created to be observed from thousands of passing vehicles. Each has developed its own legacy and passionate supporters.

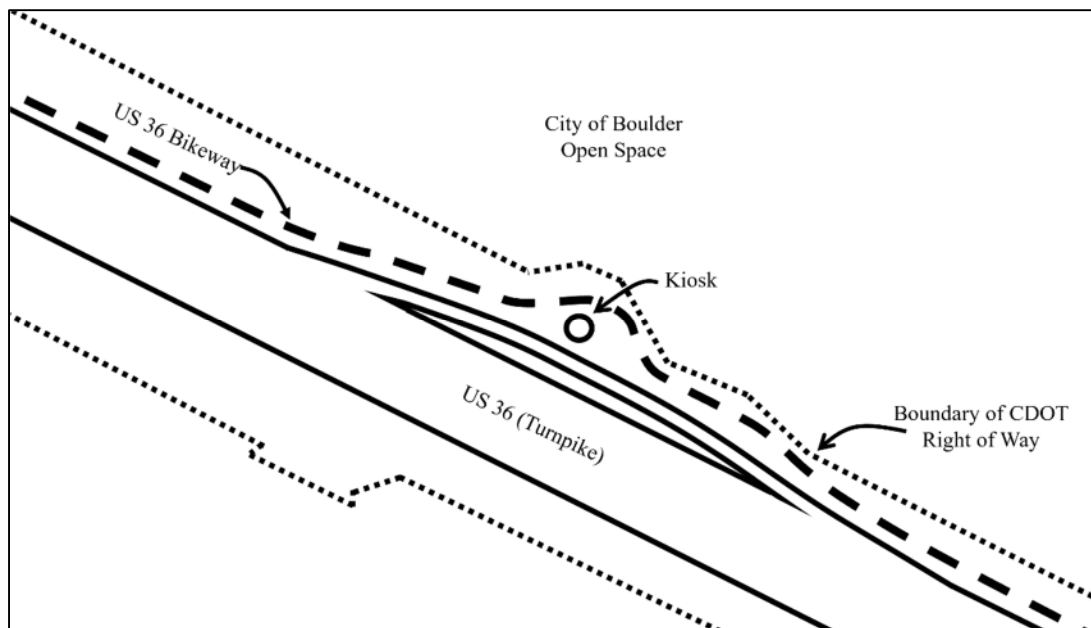
Another commonality is that both the pillars and the memorial at Legion Park have been overlooked for decades. Adding the pillars to the Legion Park site could simply exacerbate the decline of both monuments. Since the futures of the two memorials will be linked, a plan to relocate the pillars to the park should include revitalizing both and aggressively maintaining them in the coming decades.

## Relocation of the Gateway Pillars to US 36 Overlook

Perhaps the most dramatic venue in the East County is the overlook on US 36, aka the Turnpike. The vista from the overlook is spectacular, with the Boulder Valley below and the Continental Divide stretching out in the distance. Relocating the pillars to the overlook would provide them with a superb backdrop at a spot looking out on the zealously protected “frontispiece” or new entrance to Boulder. Moreover, the overlook is a site (a) where the pillars will not again be subject to further movement or disturbance, (b) which is governmentally owned, (c) where efforts could be made to enhance the pillars’ visibility as a memorial, and (d) where both pillars can be located.

### Background

The US 36 overlook is entirely located on the US 36 right of way owned by CDOT. The site is accessible only for westbound traffic from US 36 via a shallow turnout, where there is parking for approximately 8-9 vehicles. In recent years, a bikeway was built adjacent to the westbound lane of US 36, providing access to the overlook for bicyclists and walkers from both Boulder and Louisville.



*Vicinity Map - US 36 Overlook*

At present, the overlook site is small, bracketed by the turnout lane for US 36 and the bikeway. A small kiosk is the primary amenity, along with some interpretive signage, benches, and telescopes. The site has no trees or other formal landscaping, but instead is dryland prairie similar to City of Boulder open space to the north and west.

The overlook’s outstanding feature is the expansive view to the west across the valley to the Rocky Mountains in the distance. Over the past several decades, the City of Boulder spent millions of dollars protecting that view, which is often called Boulder’s “frontispiece.” Functionally, the overlook is the new gateway welcoming visitors from the east to Boulder.



*US 36 Overlook*

### **The Proposal**

The original purpose of erecting the pillars at the intersection of the Lincoln Highway and Arapahoe Road was to create a gateway to a Road of Remembrance leading visitors to Boulder. The Road of Remembrance was never built, and Arapahoe Road has been supplanted by US 36 as the primary route for visitors from the east to Boulder.

By relocating the pillars to the US 36 overlook, they again can be part of a grand entrance to the City of Boulder. The views behind them would be striking, different but comparable to the background for the original pillars. Interpretive signage and other amenities can be added to emphasize and enhance the pillars' memorial purpose.

### **Advantages**

The advantages of the proposed site are apparent:

- The background view behind the pillars on the overlook would be magnificent.
- The pillars would be visible and accessible to westbound visitors headed for Boulder, the intended audience for the original pillars.
- Though additional planning would be needed, the overlook could provide space for signage and other amenities explaining and enhancing the pillars' memorial purpose.
- The overlook provides some public parking, as well as bike access, for those who want to visit the pillars.
- Since the overlook is owned by CDOT, the process of reaching an agreement for the pillars relocation may be less complicated, and CDOT would take responsibility for the perpetual care and maintenance of the pillars and the associated signs and memorials.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those

decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

There also are obstacles inherent in the proposal:

- The overlook is situated on US 36, several miles from the “Road of Remembrance” and the pillars’ original location.
- County residents would seldom visit or even see the pillars. Instead of being an integral part of daily life in the East County, the pillars would be primarily a collateral attraction for visitors to the County enjoying the views from the overlook.
- Few passersby stop at the overlook, either from the highway or the bikeway. While the pillars would be visible to westbound traffic on US 36, their location to the side of a high-speed, heavily-trafficked expressway makes it less likely that passers-by would observe them or understand their history and purpose. Bicyclists would be more likely to observe and perhaps stop at the pillars, but their numbers are small compared to the drivers who currently use Arapahoe Road.
- The pillars would not be visible or accessible to eastbound traffic on US 36, forcing all vehicular visitors to approach in the westbound lanes.
- Those stopping at the overlook will need to cope with the noise and activity generated by traffic passing by a few feet away on US 36.
- The overlook is small and there is no obvious place to relocate the pillars given its current configuration. Accordingly, significant sitework may be needed, both to find a place for the pillars and their amenities, and to relocate some of the existing infrastructure. Depending on the site design, it might be necessary to seek a small amount of additional land from the adjacent City of Boulder open space.

### **Discussion**

In some ways, the US 36 overlook is both the best and worst alternative for the pillars. If the goal is to place the pillars in a stunning visual location where visitors can photograph them to great effect, the overlook is an ideal spot. Passersby might seldom appreciate them, yet the pillars could be publicly proclaimed to once again be the “Gateway to Boulder.”

At the same time, the pillars would lose much of their local heritage and become almost exclusively a tourist attraction. Signage and other amenities could be installed to explain their memorial purpose, but the pillars’ location alongside an expressway would make it unlikely that County residents would visit to reflect and honor those who served.

### **Recent Postings on 9milecornerpillars.com**

From: Rebecca Sulka <rsulka@gmail.com>

Date: Tue, Apr 12, 2022 at 8:37 PM

Subject: 9 mile corner Memorial pillars

To: <wdmaddme@gmail.com>

Hello,

Thank you for allowing folks to watch the meeting at the YMCA on 4/12/22 via zoom!  
I understand that one hundred years ago that intersection was intended to be a 'gateway' off the 'Lincoln Highway' to Boulder and the structures are WW1 memorial pillars.

I really appreciated hearing the story of someone who knew they were a memorial and remembers the gateway before the turnpike was built. I've lived just a few miles from them for 28 years and I'm only now learning of their significance. There was no way, even before the recent CDOT work, that I was going to get out of my car and walk around them. Knowing how special the memorial pillars are to that community member, I say this with sympathy, I'm afraid when they built the turnpike, with it's sweeping views of the Boulder valley the 'gateway' purpose of those pillars was replaced by highway 36.

That being said the memorial has not been honored as it should be, like the way that man would straighten up, turn and salute.

I agree with the idea to keep them as close to where they are as possible. My personal hope is that there could be a considerable effort to find somewhere along the "Lincoln" highway where the pillars could stand as a frame of the beautiful back range view, and that one day we could all read the names on those memorials in safety with the opportunity for deep reflection.

That's my 2 cents...

Thanks again,

Becky Sulka

303-907-8025

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From: Steve Hartmeyer <sjh17@indra.com>

Date: Thu, Apr 14, 2022 at 9:17 PM

Subject: Pillar comments post-forum

To: <wdmaddme@gmail.com>

To the Members of the Mitigation Working Group:

I'm offering my commentary as a 24-year resident of Lafayette with an interest in history, local and otherwise, as the son of a U.S. Army veteran (of Vietnam), and as a citizen who has passed by the pillars on errands or my commute nearly every day for these last 24 years. I attended the public forum at the YMCA in Lafayette out of concern for the future of the pillars, and was very gratified to learn via that forum that there are many individuals and groups that have taken an interest in trying to resolve the present situation favorably.

I would like to express my appreciation to Bill Meyer and the Boulder Rotary Club in particular for spearheading efforts to communicate with the general public about this matter, and to CDOT for soliciting community involvement in determining a course of action.

In hearing numerous constructive comments from my fellow citizens during the forum, and seeing the written alternatives offered in the pamphlet presented at the forum, my original generalized opinion of the situation has become clarified, and I would share it thusly:

First of all, it appears nearly self-evident that although preservation in-situ might be desirable to many for the historicity of the location, and potentially less expensive, in-situ "preservation" cannot possibly be considered practical.

A reconstruction of the intersection of US 287 and Arapahoe, impactful to the pillars, has just been completed. Relocation of one of the pillars previously took place in an earlier reconstruction of this intersection. There is every reason to expect that this intersection must be reconstructed again in the future due to continuing growth and changing transportation needs in the vicinity. There can and will only be more traffic at this location in the future as Erie continues to expand and the population of Boulder County grows. Furthermore, several speakers at the forum very clearly complained about the current configuration of the intersection with apparent justification, suggesting that there will be considerable impetus to have the intersection subject to reconfiguration again immediately or in the very near future, and any further adjustments are likely to be a detriment to the pillars.

Therefore, I argue there is no valid choice except to relocate the pillars, despite the potential expense.

I was pleased to see a list of suggested alternative locations expressed in the pamphlet, several of which were described in remarks by Mr. Meyer. I concur with the opinion expressed by the representative of the Daughters of the American Revolution who attended, that the pillars if relocated should be placed in a space that will permit visitors to view them close-up, and that such a space should include interpretive signage, at least similar in quality to interpretive signage common in Boulder County Open Space.

It should be obvious that such signage should include information about the pillars' former location, and the reasons for being placed there originally, and the reasons for relocation.

I very much appreciated Mr. Meyer's suggestion that members of the CU ROTC cadre attend the pillars on Veteran's Day. Although for various reasons locating the pillars on the CU campus may not be ideal, I should like to think that a relationship with the CU ROTC program could and should be fostered such that the pillars may be treated as the memorial they were meant to be, and become a site for remembrance ceremonies. When I was a Boy Scout, I recall that ceremonies of respect for veterans were occasionally attended by members of my troop, and perhaps local Girl Scout and Boy Scout troops could similarly be enlisted along with ROTC students and various local veterans' groups to volunteer to help maintain the grounds and organize appropriate ceremonies.

Several locations mentioned in the pamphlet are appealing, foremost among them in my mind possibly being Legion Park. I am not very familiar with this park except to know its location along Arapahoe Road, despite having lived in Boulder County for 29 years, but it seems to have the potential to be a quiet space conducive to remembrance and ceremony. Many other suggested locations in busier parts of the county would lack this advantage (for example, the US 36 overlook may be scenic, and a gateway, but I think constant noise from an immediately-adjacent major divided highway is hardly appropriate for peace and remembrance).



As much as I would like to recommend keeping the pillars as near as possible to their original location (now in Lafayette), I think it is most important to place them in a new location where (a) they can be decently preserved, (b) they can be readily visited by the public, (c) they can be accompanied by signage, and ideally (d) new memorials to veterans of more recent conflicts (WWII, Korea, Vietnam, Iraq, and Afghanistan were all mentioned in passing at the forum) may be added to a potential landscape or garden of remembrance.

It would seem that an existing large park or open space location would lend itself to these goals better than placement near retail or within a confined municipal public space might do.

Thank you for the opportunity to share my opinion. When it comes time to gather funding for the expenses to removing and preserving the pillars, please ensure to contact me. I pledge at this time to contribute no less than \$1000.00 toward this cause once a funding vehicle is available.

Best regards,

Steven J. Hartmeyer

209 Acadia Avenue

Lafayette, CO 80026

(303) 604-0162

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From: Barbara Torres <BTorres@care4denver.org>

Date: Tue, Apr 26, 2022, 10:06 AM

Subject: Pillar preservation

To: wdmaddme@gmail.com <wdmaddme@gmail.com>

The purpose of the pillars was not only to honor the soldiers of WWI but also to entice tourists to come to Boulder. It's important to keep in mind this DUAL purpose. The pillars were meant to be a Gateway and were put at their location for a reason. They were not meant for people to get out of their cars and have a moment/place to reflect. All of Arapahoe was the "Road of Remembrance" where people could reflect about the soldiers of Boulder County as they drove to Boulder.

The pillars aren't in anyone's way. Let's not rape the intersection like Boulder did with the 75th Street intersection.

The people who want the pillars moved are likely not Boulder County natives. They didn't drive through them to get to Denver like we used to do. They have no special ties or memories of the pillars. To move them would render them almost meaningless because they would lose half of their purpose. They would no longer be a Gateway to Boulder from 287 they were intended to be.

Barbara (Woodley) Torres

Boulder native



April 28, 2022

## **PROPOSED ALTERNATIVES FOR THE FUTURE OF THE GATEWAY PILLARS**

For the past few months, a Mitigation Working Group composed of roughly a dozen civic, veterans, and historic preservation groups from across the Boulder County have engaged in a dialogue with our local community regarding the future of the Gateway pillars located on Arapahoe Road just west of its intersection with US 287. Scores of ideas have been discussed, investigated, and debated.

The Working Group now has narrowed the list to the following alternatives which appear to be potentially feasible and have some community support (in order of distance from current location):

1. Current Location
2. Proposed Intersection of Arapahoe Road and Aspen Ridge Drive
3. Forest Park
4. Cross Ridge Park
5. Legion Park
6. US 36 Overlook

A more detailed description of each of these alternatives is attached. Over the next few weeks, the Working Group will continue to seek community input concerning, and investigate the viability of, these alternatives. Members of the community are encouraged to offer their comments and suggestions concerning these alternatives through the portal at [9milecornerpillars.com](http://9milecornerpillars.com).

The goal of the Working Group is to finalize a list of one to three alternatives, and present them this summer for final public discussion.

## **The Gateway Pillars at the Current Location**

One alternative for the pillars' future is that they remain in or near their current location. Two approaches have been suggested:

1. *Leave the pillars where they presently sit, undertake necessary repairs and reinforcement of the structures, and add signage to identify the pillars to passing motorists.*

This approach reflects the view that though the setting of the pillars has been substantially eroded over the years, the pillars in their current location remain a significant East County landmark and must be preserved *in situ*. Relocation, according to this view, destroys “what makes the pillars the pillars.” This approach also would be less expensive than any relocation options.

The primary disadvantage of this approach is that it discounts the recent findings by CDOT that the 2021 widening project severely impacted the physical integrity and historic significance of the pillars, and added a new and serious vehicular hazard for eastbound traffic. To somewhat mitigate these concerns, the following steps could be undertaken:

- Repairs and additional buttressing of the pillars and their footings could reduce the impacts of the recent construction on their physical integrity.
- The “pork chop island” where the south pillar is located could be redesigned to remove utility fixtures, add landscaping and signage, and replace the guardrails with well-designed knee walls or other more attractive protective structures. Similar changes could be made in the vicinity of the north pillar to improve its visibility and appearance.
- Removing the steel guardrails and replacing them with attractive but carefully engineered protective structures could reduce the dangers to vehicular traffic.

The net result would be a mitigation, but not elimination, of the issues reflected in the recent CDOT survey. The pillars would remain enmeshed in a complex, busy intersection with all of the impacts noted by CDOT. They are likely to be further impacted by additional construction at the intersection within the next two decades. Indeed, a Traffic Impact Study commissioned in November 2019 by the developer of Nine Mile Corner concluded that an additional left turn lane at that intersection will be required on eastbound Arapahoe Road by 2040, necessitating a further widening of that road at the current location of the pillars. Nonetheless, if keeping the pillars in their current location is deemed to be of primary importance, then these steps could moderate some of the impacts of the recent construction.

2. *Same as prior approach, but also reconfigure the intersection in some unspecified way to better protect and display the pillars.*

Recognizing that the prior option does not resolve the serious impacts of the 1983 and 2021 widening projects on the Gateway's historic setting, it has been suggested that the intersection be reconfigured to somehow restore the original appearance of the pillars and the surrounding area. Multiple, additional objections to the current intersection design – many of which have nothing to do with the pillars – also have been raised.

Unfortunately, after lengthy discussions, no alternative design has been proposed that is compatible with the pillars' historic setting, while at the same time providing a safe and efficient

intersection for current and anticipated traffic flows. Even if one could design the intersection on a blank sheet of paper, with the only requirements being that (a) the pillars be kept in or near their original locations, and (b) the intersection safely and efficiently handle the anticipated volume of traffic, it seems unlikely that a viable alternative configuration could be developed. Nonetheless, parties are encouraged to propose any designs that meet these criteria.

## **Relocation of the Gateway Pillars to the Intersection of Arapahoe Road and Aspen Ridge Drive**

A major challenge in finding a solution to the current situation with the Gateway pillars is developing plan that (a) maintains some semblance of their original setting, (b) is feasible, and (c) provides for their future. The following are two proposals that might meet these goals.

### **Background**

Many community members have commented that the original location of the pillars – which was preserved for nearly a century – both marked the symbolic entrance to the City of Boulder and formed a visual gateway to Arapahoe Road framed by open farmland and the mountain backdrop to the west. Much of that character has been lost with the two intersection widening projects in 1983 and 2021. Arapahoe Road west of the current location, however, retains some of its original 1928 character. That is, it is a narrow two-lane road predominantly flanked by open land, which still offers sweeping mountain vistas to the west. Moving the pillars a few hundred feet to the west could recapture much of their original character.

But the nature of that portion of Arapahoe Road will soon change. The City of Lafayette has approved construction of a development currently known as the Silo subdivision on the south side of Arapahoe beginning about a quarter mile west of the pillars' current location. Another commercial development is being planned by the Tebo Partnership, LLP for the land on the south side of Arapahoe between the pillars and the Silo site.

To accommodate those two developments, a new arterial – to be known as Aspen Ridge Drive – is planned for the south side of Arapahoe Road along the boundary between the properties. Current plans call for installation of a new traffic light at the Arapahoe Road – Aspen Ridge Drive intersection, roughly ¼ mile west of the Arapahoe Road – US 287 intersection.

Moreover, it also appears probable that traffic flows soon will require that Arapahoe Road be widened. A Traffic Impact Study commissioned in November 2019 by the Nine Mile Corner developer predicts that traffic on Arapahoe between US 287 and 95<sup>th</sup> Street will increase by 35% 2021 and 2040. Such increased traffic flows presumably will increase the pressure to widen that portion of Arapahoe Road to four lanes.

At the same time, both Boulder County and the City of Lafayette are taking steps to preserve the semi-rural character of many of the properties along Arapahoe. Even the planned developments, though obviously changing the appearance of those tracts, include setbacks, open space, and other features adjacent to Arapahoe Road that will temper the urban feel of the area. As a result, Arapahoe Road will retain some of its rural character and most of its mountain views for westbound traffic coming from the US 287 intersection.

Because the reconstruction of Arapahoe Road in the vicinity the new developments has not yet begun, there may still be time to influence the design of the Arapahoe Road – Aspen Ridge Drive intersection to incorporate a new home for the Gateway pillars. Below is a proposal for such a redesign, which recreates some semblance of the pillars' original setting in a location unlikely to be impacted by additional future changes to Arapahoe Road. In addition, a less ambitious option for placing the pillars in a park near the intersection is discussed.

## **Proposal A – A Traffic Circle at the Intersection of Arapahoe Road and Aspen Ridge Drive**

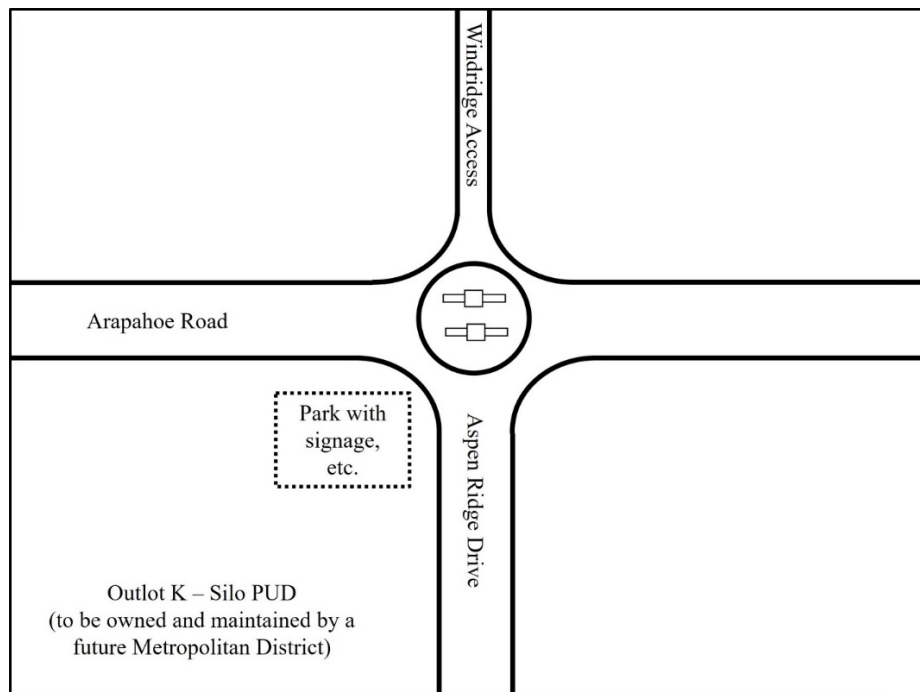
The primary stumbling block to recreating a new “gateway” using the pillars on Arapahoe Road is the likelihood that the highway will be widened so that (a) the pillars must again be moved, and/or (b) their gateway character will be lost. One answer to that obstacle is to route Arapahoe Road around the pillars. In that fashion, if Arapahoe Road is again widened in the future, the new width could be added to the outer lanes without impacting the pillars.

This design could be accomplished by constructing a traffic circle at the intersection of Arapahoe Road and Aspen Ridge Drive, with the pillars relocated in the middle of the traffic circle. Simple signage identifying the pillars for drivers could be placed on the circle, with perhaps some landscaping, lights, or other features.



*Exemplar traffic circle – Erie Parkway and 119<sup>th</sup> St.*

While the traffic circle would not be readily accessible by foot, a small park could be constructed on the SW corner of the intersection with more detailed interpretive signage, historic photos, etc. This park could be accessed on foot from either the Silo or Tebo developments.



*Concept for Relocation to Arapahoe Road – Aspen Ridge Drive Intersection*

## *Advantages*

The proposed design, if adopted, would have several advantages:

- Those in vehicles approaching the traffic circle on Arapahoe Road would see the pillars directly ahead, not off to the side. While vehicles would not pass between the pillars, the view from the east and west would preserve and highlight their historic gateway character.
- Approaching from the east, the pillars' backdrop would be open land with mountains in the distance, approximating the view down a Road of Remembrance planned in 1928.
- The pillars will be enjoyed by thousands of motorists each day since they are squarely in view, not relegated to a side venue where special effort is required to see them.
- Passing motorists primarily will be the same Boulder County residents who currently enjoy the pillars, many with an attachment and appreciation for their history.
- The adjacent park would provide space and opportunity to erect signage to explain the pillars' history and significance. If appropriate, the park could be used to expand the memorial purpose of the pillars to honor veterans of other wars.
- Compared to a traffic signal, a traffic circle would enhance the appearance of Arapahoe Road and the entrance to the two developments.
- The current status of the intersection of Arapahoe Road and Aspen Ridge Drive presents an opportunity to design and build such a configuration from scratch, rather than retrofitting or squeezing into an existing site.
- By including the traffic circle in the initial construction of the intersection, the expense of some of the sitework could be included in the cost of the highway improvements.
- The pillars and park could potentially be owned by a governmental entity, thus providing for perpetual care and maintenance.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

## *Disadvantages*

As one might expect, there also are obstacles inherent in the proposed design:

- The project is ambitious, requiring the consent and cooperation of multiple parties.
- The timeline for completing the new Arapahoe Road – Aspen Ridge Drive intersection is uncertain, but may be several years away.

- The new Arapahoe Road – Aspen Ridge Drive intersection will be located at the top of a slope on a highway currently with a 50-mph speed limit. There may be concerns about placing a traffic circle in such a location. On the other hand, those same issues will arise regardless of what design is adopted for the new intersection. The expertise of traffic engineers will be needed to determine the nature and extent of any additional danger posed by a traffic circle.

- There may be concerns about whether a traffic circle would interfere with traffic flows on an already congested Arapahoe Road. On the other hand, a traffic circle might improve the efficiency of traffic movement compared to the currently planned signal. These relative efficiencies should be addressed by traffic engineers to determine the feasibility of the proposal.

- No pedestrian access to the pillars will be provided. However, the original Gateway was designed to be viewed from passing vehicles; even the signage (plaque, cornerstone) was designed to be seen from a passing vehicle. This design retains that concept, plus the adjacent park will have pedestrian access, where explanatory signage or memorials can be placed.

### ***Discussion***

Though multiple details of this plan need addressing, some are discussed below:

*Land acquisition for the traffic circle:* Ownership of the land needed for a traffic circle, put mildly, is a complex issue:



*Land Ownership at Proposed Arapahoe Road – Aspen Ridge Drive Intersection*



A traffic circle would be built principally on the right of way for Arapahoe Road (CO 7), which is owned by CDOT. But regardless whether the pillars are relocated to the Arapahoe Road – Aspen Ridge Drive intersection, it appears probable that some additional right of way will be required for that intersection and/or a widened Arapahoe Road. If the proposed traffic circle can be constructed within the existing and already planned right of way, then no additional land will be required.

Until the final design of the intersection is completed, it is impossible to know if additional land will be required. Nonetheless, while it appears that much of the traffic circle and surrounding traffic lanes can be accommodated in the existing right of way, the geometry of the proposed traffic circle and surrounding traffic lanes probably will require that small amounts of additional right of way be acquired from each of the four landowners bordering the new intersection (see map above).

The southwest corner. The land on the southwest corner of the new intersection is currently owned by Hearteye Village Land Company, which in 2017 entered into a PUD for the Silo development with the City of Lafayette. That PUD includes a plan for the construction of Aspen Ridge Drive along the eastern boundary of the Silo subdivision where it will intersect with Arapahoe Road.

In addition, that PUD designated Outlot K on the northeast corner of the Hearteye property – some of which might be needed for a traffic circle – to be used as a park. According to the PUD, this park will be owned and maintained by the Weems Neighborhood Metropolitan District (WNMD), a quasi-governmental entity encompassing the Silo subdivision (see above diagram). However, Hearteye has not yet transferred Outlot K to WNMD.

Hearteye later agreed to sell the Silo subdivision to Cornerstone Homes Development Company. Hearteye and Cornerstone subsequently entered into a Development Agreement with the City of Lafayette on May 2, 2019. That Agreement provides that the Silo project would be developed in two distinct phases, with each phase occurring over an approximately ten-year buildout period. Phase 1 would develop the southern portion of the property, leaving the northern portion abutting Arapahoe Road for Phase 2.

In addition, the Development Agreement made specific reference (§27b) to the installation of a traffic signal at the Arapahoe Road – Aspen Ridge Drive intersection. That Agreement provided that Cornerstone would install a traffic signal at that intersection in accordance with CDOT specifications. The precise timing of the construction of the new intersection is not stated, since it depends on other factors specified in that Agreement.

The Agreement further provided that Cornerstone would be responsible for 50% of the cost of the traffic signal, while the owner of the adjacent Tebo Partnership property should be responsible for the remaining 50%. However, if CDOT required construction of the traffic signal before the contribution by the owner of the Tebo Partnership property could be secured, Cornerstone would be responsible for 100% of cost, subject to a right of later reimbursement. The Development Agreement also contains a number of other specific provisions related to the development of the land in Phase 2 adjacent to Arapahoe Road.

On May 3, 2019, Cornerstone sold the land for Phase 1 to Lennar Colorado, LLC, and assigned its rights and obligations under the Development Agreement to Lennar. At the same time, the various parklands, open space, and other land designated on the PUD for WNMD in Phase 1 – but not Phase 2 – were transferred to WNMD.

Accordingly, at the present time, it appears that:

- Hearteye remains the legal owner of the land for Phase 2, including Outlot K on the northeast corner of the Silo development. However, per the PUD, when Phase 2 is developed, Hearteye is obligated to transfer Outlot K to WNMD. With the agreement of the parties, any land needed for the traffic circle and/or the memorial park could be set aside prior to that transfer.
- Lennar, as assignee of the Development Agreement, is obliged to construct the traffic signal at the Arapahoe Road – Aspen Ridge Drive intersection. Since work on that intersection has yet begun, funding earmarked for the signaled intersection could be used to construct a traffic circle.

The southeast corner. The Silo PUD and other City of Lafayette planning documents anticipate that the Arapahoe Road – Aspen Ridge Drive intersection will be a primary access for any development on the Tebo Partnership LLP property. The Development Agreement for the Silo subdivision accordingly anticipates that the Tebo Partnership LLP or its successor will contribute 50% of the cost of the intersection improvements. However, it appears that no agreement has been reached between the City of Lafayette and the Tebo Partnership LLP concerning those matters. Again, if the parties agreed, funds required to construct the signaled intersection could be repurposed to construct a traffic circle.

The northeast corner. The land on the northeast corner of the new Arapahoe Road – Aspen Ridge Drive intersection is owned by Stephen Tebo, an owner of Tebo Partnership LLP which owns the property directly south across Arapahoe Road. Though the plans for the northern parcel are not known, it seems possible that an arrangement might be negotiated with Mr. Tebo to acquire a small amount of additional land for the traffic circle from that parcel, if needed.

The northwest corner. The parcel adjacent to the northwest corner of the new intersection includes a substantial residence and agricultural operation. The driveway for that property is directly across Arapahoe Road from the planned entrance and traffic signal at Aspen Ridge Drive (see above diagram). The property was owned by Richard P. Miller, who recently passed away and it appears that the property is currently controlled by his estate while his affairs are being wound up. Though any discussions concerning the acquisition of land for the proposed traffic circle could be complicated by the estate proceedings, Mr. Miller's estate also has some incentive to have the intersection plans finalized prior to any transfer of the Miller property.

Moreover, the Miller property is subject to what was known as the Windridge PUD, which required the landowner to execute a conservation easement in favor of Boulder County on the land adjacent to Arapahoe Road. While the County cannot unilaterally transfer a portion of that land for intersection improvements, its easement rights lessen the value of the land for the Miller estate.

Conclusion: Assembling the land for a traffic circle would be a complicated, time-consuming process. On the other hand, that amount of land at issue for each landowner should not be significant, and would include land already significantly impacted by the planned signaled intersection. Further, the adjacent landowners may embrace the new design, since replacing a traffic signal with a traffic circle could benefit their properties.

*Land acquisition for the Park:* The proposed park on the SW corner of the intersection would not be located in the CDOT right of way, but would be developed on a portion of Outlot K in the Silo subdivision. Since the PUD designates Outlot K as a small park to be owned and maintained by WNMD, the placement of memorial signage or other similar items relating to the Gateway pillars in that park would seem to be compatible with those plans.

*Design:* If the traffic circle proposal is pursued, community input should be solicited regarding the various design elements. One appealing design envisions the pillars situated on an east-west axis and standing parallel, similar to their original configuration, but only a few feet apart. A paved, brick, or stone path could run between them on the traffic circle, representing the Road of Remembrance. Signs on the traffic circle could identify the pillars and their memorial purpose. If appropriate, the originally planned external and internal lighting could be reworked to highlight the pillars at night.

Careful engineering will be needed to provide footings, structural reinforcement, and other design features to mitigate any damage from the vibrations from passing vehicles. Similarly, a knee wall or other low, unobtrusive barrier – similar to those used on many other traffic islands (see photo, above) – may be needed on the circumference of the traffic island to provide protection from errant drivers. Since the pillars presumably would be erected on slightly elevated footings for drainage purposes, such a wall should not obstruct the view from passing vehicles.

There are countless possibilities for the adjacent memorial park. Various signs, historical photos, plaques, or memorials could be installed. Like the original Gateway park, the new park could have a flagpole and perhaps even a replica canon. Depending on community sentiment, the park could honor only those who served in WWI or a broader segment of the veteran community.

*Ownership and future maintenance:* Under this proposal, the Gateway pillars would be located on a traffic circle on a CDOT-owned right of way, and the adjacent memorial park would be on property owned by WNMD. Both the City of Lafayette and Boulder County also have interests in the Gateway pillars and the area.

Logically, the maintenance obligations for both the pillars and the adjacent park should be consolidated under one governmental entity. While WNMD is a possibility, the precise contours of such an arrangement will need to be negotiated between the relevant authorities and memorialized in an intergovernmental agreement or similar instrument.

### **Proposal B – Memorial Park at the Intersection of Arapahoe Road and Aspen Ridge Drive**

A less ambitious alternative would be to relocate the pillars to the planned park on Outlot K of the Silo subdivision on the southwest corner of this intersection (see diagram, above). As previously discussed, that land is currently owned by Hearteye but, under the terms of the Silo PUD, Outlot K is slated to be transferred in Phase 2 to WNMD for use as a park. Particularly since

that area is still in the design stage, it seems feasible to discuss relocating the Gateway pillars to the planned park.

### ***Advantages***

This alternative, if adopted, would have several advantages:

- The pillars would be visible to thousands of vehicles approaching the intersection on westbound Arapahoe Road, roughly ¼ mile from their original location.
- Passing motorists primarily will be the same Boulder County residents who currently enjoy the pillars, many with an attachment and appreciation for their history.
- The park should provide space and opportunity to erect signage to explain the pillars' history and significance, and pedestrian access to the pillars and the amenities. If appropriate, the park could be used to expand the memorial purpose of the pillars to honor veterans of other wars.
- The current status of the intersection of Arapahoe Road and Aspen Ridge Drive presents an opportunity to design and build such a park from scratch, rather than retrofitting or squeezing into an existing site.
- By including the park in the initial construction of Phase 2 of the development, the expense of some of the sitework could be included.
- The pillars and park could potentially be owned by a governmental entity, thus providing for perpetual care and maintenance.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### ***Disadvantages***

There also are obstacles inherent in the proposed design:

- The pillars will be used solely as a monument, losing their unique gateway character and with less visibility than the present location for westbound traffic on Arapahoe Road.
- The pillars will have little visibility for eastbound traffic on Arapahoe Road.
- The timeline for completing Phase 2 of the Silo development is uncertain, but may be several years away.
- Outlot K is traversed SW to NE by the Davidson-Highline Lateral Ditch, as well as planned stormwater and other easements. While smaller structures such as signage or benches likely should not interfere with those uses, significant engineering and planning would be required

to place structures the size of the pillars on that tract. A preliminary review suggests that the pillars would need to be relocated some distance from Arapahoe Road, decreasing their visibility from passing vehicles.

- Though Phase 2 has not been platted, under the design reflected in the PUD does not include an public parking in the vicinity of Outlot K, potentially raising conflicts with the residents of the development.

### **Conclusion**

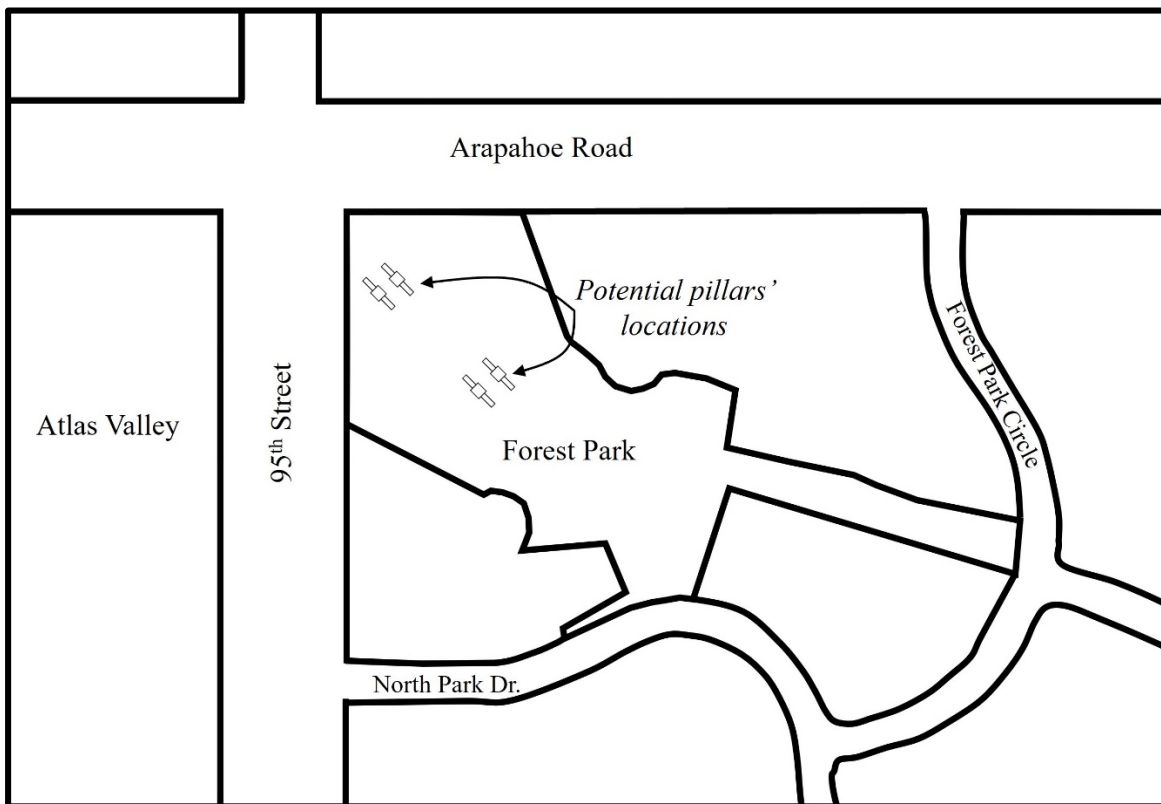
Decades of neglect have allowed the purpose and setting of the Gateway pillars – a community memorial to those who served in WWI – to be lost in the sprint for development. Proposal A cannot return the clock to 1928, but may offer a path for reclaiming at least part of the community’s vision and for honoring those who sacrificed during that horrible conflict. If that idea is not implemented, Proposal B may offer the opportunity to relocate the pillars to a park close to their original location.

## Relocation of the Gateway Pillars to Forest Park

Recent community comments outlined a consensus that a future home for the Gateway pillars should be to a site (a) where they will not again be subject to further movement or disturbance, (b) which is governmentally owned, (c) where efforts can be made to enhance the pillars' visibility as a memorial, and (d) where both pillars can be located. Community members also have expressed a preference that the pillars be relocated to a site as near as possible to the current location, and with reasonable visibility for persons in vehicles passing by on a nearby highway or street. Relocation of the pillars to Forest Park at the southwest corner of Arapahoe Road and 95<sup>th</sup> Street generally meets these criteria.

### Background

Forest Park is currently owned by the City of Lafayette. The parcel is slightly more than 4 acres in size, and was created as Outlot F when the Forest Park subdivision was approved in 1998.



*Forest Park Location Map*

Though the park is owned by the City of Lafayette, it is maintained by the Forest Park Village homeowner's association. The landscaping is mature with grass open spaces and numerous mature trees. The area along the north boundary of the park adjacent to Arapahoe Road is sunken below the grade of the adjacent roadway to serve as a catch basin for storm runoff.



*Forest Park from the southeast corner of Arapahoe Road and 95<sup>th</sup> Street*

### **The Proposal**

The location and ownership of Forest Park make it a viable site for relocating the Gateway pillars as part of a memorial to soldiers who served in WWI, and potentially including those who served in other conflicts. While the relocated pillars likely would not be easily seen on Arapahoe Road, the park is within the general vicinity of the pillars' original location and provides ample space for creating a memorial featuring the pillars along with signage, landscaping, and other features.

### **Advantages**

The proposed design, if adopted, would have several advantages:

- The park is situated on Arapahoe Road, the planned "Road of Remembrance," roughly 1½ miles from the pillars' original location.
- The park's location is most visible to eastbound traffic on Arapahoe Road and southbound traffic on 95<sup>th</sup> Street. Depending on the location of the pillars within the park, they may or may not be visible to this traffic.

- The park would provide ample space to create a memorial park among the mature trees in a pleasant setting, with the pillars as a centerpiece accompanied by explanatory signage and other amenities.
- The nearby parking lot for Forest Park Village provides public parking for those who want to visit the pillars.
- Compared to other alternatives, this site may be less expensive since the park is accessible with some infrastructure in place.
- Since the park is owned by the City of Lafayette, the process of reaching an agreement may be less complicated.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

There also are obstacles inherent in the proposal:

- Regardless where the pillars are sited in the park:
  - They will lose their unique gateway character, and
  - They will have little visibility for westbound traffic on Arapahoe Road.
- If the pillars are relocated to the interior of the park away from the northwest corner to create a memorial park, their visibility from the roadways is essentially eliminated.
- If the pillars are relocated to Forest Park, mature landscaping will need to be removed. The City and homeowner's association likely would not be supportive of removing existing mature trees, and would require replacing them with trees of a matching caliper. The City probably also would require a formal landscape plan, prepared by a landscape architect, that would include a new irrigation plan to accommodate those changes.
- The commercial interests in Forest Park Village may object to (or may like) the possibility that the memorial park could bring additional visitors to park in the commercial spaces.
- The current arrangement for provides for the Forest Park Village homeowner's association to maintain the park. If the pillars are moved to Forest Park, some modification of that arrangement may be required, and the City would need to agree to be responsible for the perpetual maintenance of the pillars and any amenities.



## **Discussion**

If the decision is made to create a memorial park featuring the Gateway pillars in the general vicinity of the current location, then Forest Park is a viable option. Since the property is owned by the City of Lafayette, discussions for creating such a park could involve fewer parties and less legal complication. Moreover, though the presence of mature trees and landscaping may pose obstacles, they also could provide a peaceful setting for the memorial.

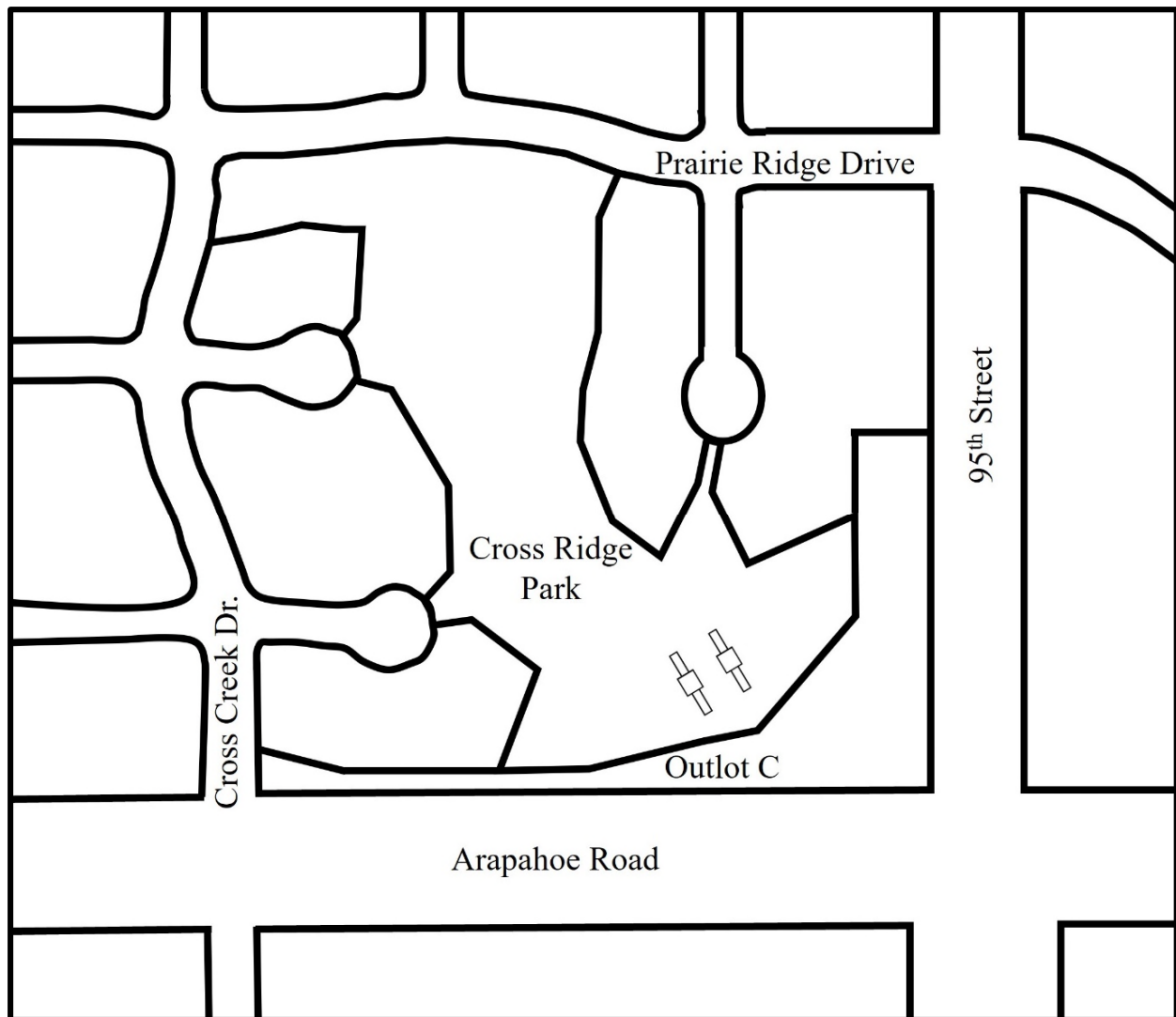
The primary decision for the community is whether to substantially change the nature of the pillars' setting, and use them as the centerpiece for a pedestrian-accessed memorial park rather than as a monument designed to be primarily viewed from a vehicle. If the former is selected, then Forest Park may be a good choice.

## Relocation of the Gateway Pillars to Cross Ridge Park

Like Forest Park and some other alternative locations, Cross Ridge Park could serve as a site for relocating the pillars. The site (a) likely will not be impacted by future road expansions, and thus would avoid further movement or disturbance of the pillars, (b) is governmentally owned, (c) has space to add amenities to enhance the pillars' use as a memorial, and (d) can accommodate both pillars. Cross Ridge Park also is relatively near the current location, and with some visibility for persons in passing vehicles.

### Background

Cross Ridge Park is a 4.74 acre neighborhood park managed by the City of Lafayette. The park was created in 1994 as part of the Cross Creek subdivision.



*Concept for Relocation to Cross Ridge Park*

The bulk of the park is an open, grassy area. The portion nearest to the corner of Arapahoe Road and 95<sup>th</sup> Street is significantly below the grade of the adjacent roadways, and serves as a catch basin for storm runoff.



*Cross Ridge Park – view from NW towards Arapahoe Road – 95<sup>th</sup> St. Intersection*

Between the park and the roadways on the west and south is a strip of land designated as Tract C in the Cross Creek subdivision filing, which is owned by the Ridge at Cross Creek Homeowners Association. The portion of this strip on the corner of Arapahoe Road and 95<sup>th</sup> Street is landscaped with mature trees and contains part of a path that circumnavigates the park.

### **The Proposal**

The location, orientation, and ownership of Cross Ridge Park make it a viable site for relocating the Gateway pillars. While there are a number of possible locations for pillars in the park, one site would be at the southeast end of the park west of the mature trees and as close as feasible to the northwest corner of Arapahoe Road and 95<sup>th</sup> Street. In that location, the pillars could be oriented on a somewhat NW-SE axis so that the “gateway” between them is visible to westbound traffic. In addition to the pillars, there is ample room in that area of the park for signage, landscaping, and other features that would be somewhat visible to westbound traffic and accessible via the existing walking path in the park.

### **Advantages**

The proposed design, if adopted, would have several advantages:

- The park is situated on Arapahoe Road, the planned “Road of Remembrance,” roughly 1½ miles from the pillars’ original location.
- The park’s location on the northwest corner of the Arapahoe Road – 95<sup>th</sup> Street intersection is somewhat visible to westbound traffic on Arapahoe Road, the original intended audience for the Gateway pillars. Depending on the location, the pillars could also be somewhat visible to north and/or southbound traffic on 95<sup>th</sup> Street.
- The motorists viewing the pillars primarily will be the same Boulder County residents who currently enjoy them.
- The park would provide ample space and opportunity to erect signage to explain the pillars’ history and significance. If appropriate, the park could be used to expand the memorial purpose of the pillars to honor veterans of other wars.
- The combined park and outlot contain open spaces and a walking path, reducing the amount of work necessary to prepare the site.
- If sited in the existing park, the pillars and park would be owned by the City of Lafayette, which would agree to be responsible for perpetual care and maintenance.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

The obstacles inherent in the proposal are as follows:

- If the pillars are relocated into the existing park, they would:
  - Be some distance from and substantially below the grade of Arapahoe Road, reducing their visibility to passing motorists, and
  - Lose their unique gateway character.
- Regardless of the siting, the pillars will have poor to non-existent visibility for eastbound traffic on Arapahoe Road.
- The siting of the pillars is complicated by the fact that it is a detention pond converted into a park. The park receives all the storm drainage for that neighborhood, so that the southeast corner of the park holds water during many weather events and even during snowmelt.

The flat area of the park has an engineered and installed drainage system. Any changes to that area likely would require that both the drainage and irrigation systems be re-engineered and rebuilt.

- To relocate the pillars close to the corner of the intersection, some agreement would be required to transfer a portion of Outlot C to the City of Lafayette.
- The siting of the pillars is complicated by the presence of mature trees on Outlot C close to the corner of the intersection. If the pillars are sited behind the mature trees, or west of the corner to avoid the trees, the pillars' visibility from the roadways will be further reduced.
- Cross Ridge Park was designed as a neighborhood park, with little adjacent parking. Residents in the Cross Creek subdivision should be consulted regarding the addition of the pillars and associated amenities to that park, and likelihood of additional traffic in the neighborhood.

### **Discussion**

In comparison to some alternatives, relocating the Gateway pillars to Cross Ridge Park would involve fewer parties and less legal complication. If the City of Lafayette agreed to the proposal, the legal requirements and approvals would be significantly reduced, and the project simplified. Those issues could be more difficult if the pillars were moved partly or entirely on to a portion of Outlot C currently owned by the Cross Creek Homeowners Association.

Though the park is relatively open land, the site poses significant engineering complications given its use as a stormwater catch basin. The presence of mature trees on the corner, and the tract's use as a neighborhood park could inject additional issues.

Finally, relocating the pillars into a park, even if visible from a roadway, changes their character and the public's perception of them. While their history could be explained – likely better than it is explained now – the pillars would become more of a static memorial than part of the everyday fabric of the East County.

## **Relocation of the Gateway Pillars to Legion Park**

Of all of the potential alternative sites for the pillars, Legion Park aligns most closely with the original memorial purpose of the pillars. Dedicated in 1934, Legion Park was created as “a memorial to the soldier dead of the World War.” Though never formally associated with the Road of Remembrance, the park accordingly shares the same memorial purpose as the Gateway pillars. Moreover, Legion Park meets other criteria identified by the community including that it is a site (a) where the pillars will not again be subject to further movement or disturbance, (b) that is governmentally owned, (c) where efforts can be made to enhance the pillars’ visibility as a memorial, and (d) where both pillars can be located.

### **Background**

In 1917, a local Boulder businessman, John Howard Empson, deeded 28 acres to Boulder County, including Goodview Hill on the north side of Arapahoe Road overlooking the Boulder Valley. The parcel was later reduced in size to ~23 acres when the County dedicated a strip along the south boundary for the newly build Arapahoe Road.

In 1931, Boulder County commissioners requested the construction of a new scenic road to the top of Goodview Hill to provide a vantage point from which visitors will get an “unparalleled panoramic view.” When this opportunity arose, Post 10 in 1932 leased the Goodview Hill property from the County “for the purpose of beautifying said tract as a memorial to the soldier dead of the World War.” The lease, which bears a rent of 50¢ per year, expires on October 31, 2031.

Thereafter, the memorial was designed by Saco Rienk DeBoer, a noted landscape architect from Denver. His design for the memorial called for 2’ high rock wall, 382 feet long, and 100 feet wide with flagpoles located at each end, located at the top of Goodview Hill. Two captured artillery pieces (from the same source as the cannon at the Gateway park) were installed on platforms near the flagpoles. Two elaborate entrance gateways were planned for the entrance road off Arapahoe Road.

A crew from the Civilian Conservation Corps constructed the hilltop memorial, but the entrance gateways were never built. Legion Park was dedicated in late June 1934, and maintenance responsibilities were turned over to Legion Post 10. However, a later study concluded: “It is unclear what work, if any, occurred on the property after the American Legion signed the lease since the country was fast approaching the lowest point of the Great Depression.”

Over the years, the park fell into significant disrepair. Legion Post 10 told the *Daily Camera* that they tried for years to maintain the park, but the vandalism remained rampant. Visitors stole the flagpoles and plaques. The Legion provided garbage cans, but they were stolen. The legion Commander was quoted as saying “we put out trash bags, but some of the bums slept in them.”

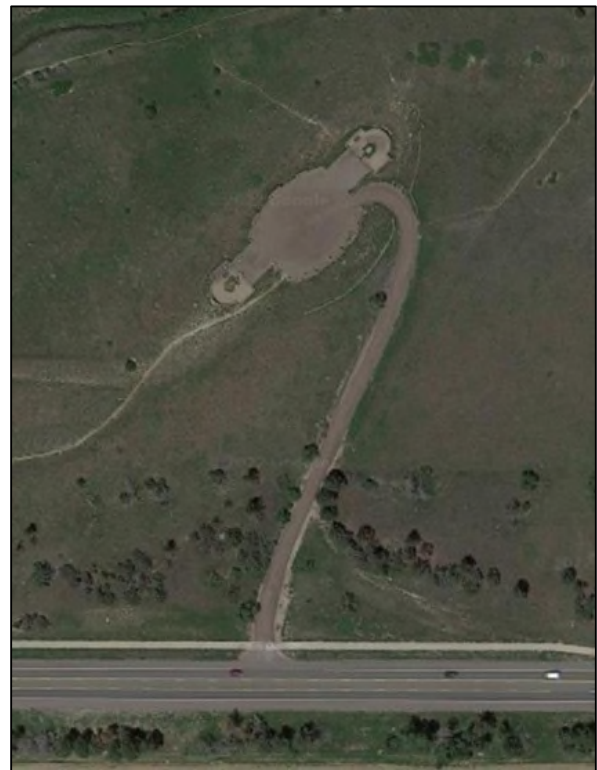
In early 1976, due to the County’s concern of the ability of Post 10 to properly maintain the park, Boulder County Parks and Open Space Department (BCPOS) took control of the park’s maintenance, though the Legion continued its sponsorship of the memorial. Toward that end, the County signed a Memorandum of Agreement with the Legion confirming their March 1, 1933 lease, but providing that the County would take responsibility for the care of the park. In this same

MOA, it was agreed that the Legion would maintain all war memorials, flag poles, plaques, and signs, continue to conduct memorial services, erect fitting displays in memory of the war dead, install additional war memorial monuments at its expense. The Legion also was given the right to formally name the park.

On Memorial Day, May 30, 1977, the Legion rededicated the park as “Boulder Veterans Memorial Park.” However, the park continued to decline. One of two artillery pieces disappeared, apparently in the 1970s. In June 2005, the Legion arranged for the Colorado Air National Guard to remove the other artillery piece, which the Legion restored and now displays at its building on North Broadway in Boulder.

Though officially named “Boulder Veterans Memorial Park,” the site is now referred to Legion Park, and Goodview Hill is now called “Legion Hill,” by the County and on most maps. However, since the last artillery piece was removed in 2005, it does not appear that Legion Post 10 has conducted any activity or done any further maintenance or improvement to the site.

Instead, County publications list Legion Park as one of its “open space properties” managed by BCPOS. The memorial at the crest of Goodview Hill been reduced to the perimeter rock wall; all of the other memorial features (artillery pieces, flagpoles, plaques) have been removed. The interior space of the memorial now serves as a parking lot for those seeking to enjoy the view, with picnic benches, two Parks and Open Space interpretive signs, and trash and recycling containers maintained by the County.



*Legion Park*

The remainder of the 23-acre property is dryland, with a 0.9 mile loop trail used for hiking, mountain biking, and other recreational purposes. It is maintained as non-irrigated open space with native or invasive prairie vegetation. Apart from the access road from Arapahoe, a “Legion Park” sign, and the loop trail, the remaining property is vacant.

On October 8, 2019, the Board of County Commissioners approved a request from BCPOS to designate Legion Park as a Boulder County Historic Landmark. This designation covers the entire 23 acres of the park, and provides that “Any construction or alteration of the landscape or features within the site area will require review and approval of a Certificate of Appropriateness.”



## **The Proposal**

During the community discussions concerning the future of the Gateway pillars, there has been significant discussion about relocating them to Legion Park. To date, however, there has been no consensus concerning the exact siting of the relocated pillars within the park. Among the locations that have been discussed are:

1. Flanking the entry drive at the bottom, set back a short distance from Arapahoe Road.
2. Flanking the entry drive about 100' – 150' up the hillside from Arapahoe Road.
3. Flanking the entry drive at the top of the hill at the entry to the memorial.



*Entry to Legion Park from Arapahoe Road*

4. On top of the hill, on the west side of the memorial's wall, adjacent to (or as part of) the existing wall.
5. On top of the hill, on the southwest corner of the memorial's wall, close to an existing wall, potentially moving up the hill.



*Current Memorial at Legion Park*



Depending on which location is selected, benches, interpretive signage, and other amenities could be installed.

### **Advantages**

The proposed location, if adopted, would have several advantages:

- Legion Park was created at roughly the same time as the Gateway by some of the same individuals and organizations that created the pillars, and shares the same memorial purpose with the pillars.
- Legion Park is situated on Arapahoe Road, the planned “Road of Remembrance,” albeit 4½ miles from the pillars’ original location.
- If sited to flank the entry road, the pillars would be visible to traffic on Arapahoe Road and continue to serve as a “gateway.”
- If sited on top of the hill, the area would provide ample space for explanatory signage and other amenities.
- The existing space at the top of the hill provides public parking for those who want to visit the pillars.
- Boulder Legion Post #10 has indicated a willingness to work with the Mitigation Working Group if the decision is made to relocate the Gateway pillars to Legion Park. Since the park is owned by Boulder County, if an agreement can be reached, the County could assume responsibility for the perpetual care and maintenance of the pillars and the associated signs and memorials.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

There also are obstacles inherent in the proposal:

- Legion Park is situated several miles from the pillars’ original location, where they were an integral part of daily life in the East County.
- Legion Park is scarcely known in Boulder County, and predominantly is used by residents of the City of Boulder. While moving the pillars to Legion Park may increase the park’s use and visibility, the pillars will be seen far less at that location than at their current or some other relocation sites.
- The park has a history of vandalism, and increased security and/or maintenance may be required.

- If the pillars are sited to flank the entry road:
  - A significant amount of earthmoving and site preparation would be required due to the slope; and
  - Explanatory signage and other amenities likely would need to be located at some distance from the pillars.
- If the pillars are sited on top of the hill, they would not be visible from Arapahoe Road and would not serve as a gateway.
- The entirety of Legion Park is designated as both a Boulder County Historic Landmark and as Boulder County open space.
  - The landmark designation is particularly important with respect to the memorial, which is protected both as a representative project constructed by the Civilian Conservation Corps and as a representative work of master landscape architect Saco Rienk DeBoer. The incentive to protect the historic integrity of the Legion Park memorial is significant, since that iconic landmark should not be degraded in the same way as the setting for the Gateway has been destroyed.
  - Boulder residents' well-known fierce protection of open space suggests that any relocation, particularly one that requires sitework which disturbs open space land, may encounter community resistance.
- If the Gateway pillars are relocated to Legion Park, they will lose their independent identity and simply become another structure in the park.

### **Discussion**

Legion Park in many respects is an attractive future home for the Gateway pillars. The park's temporal, architectural, and commemorative symmetry with the Road of Remembrance's Gateway is obvious. But the two memorials were created to be, and have been maintained for nearly a century as, independent sites. One was an impressive destination shrine, while the other was specifically created to be observed from thousands of passing vehicles. Each has developed its own legacy and passionate supporters.

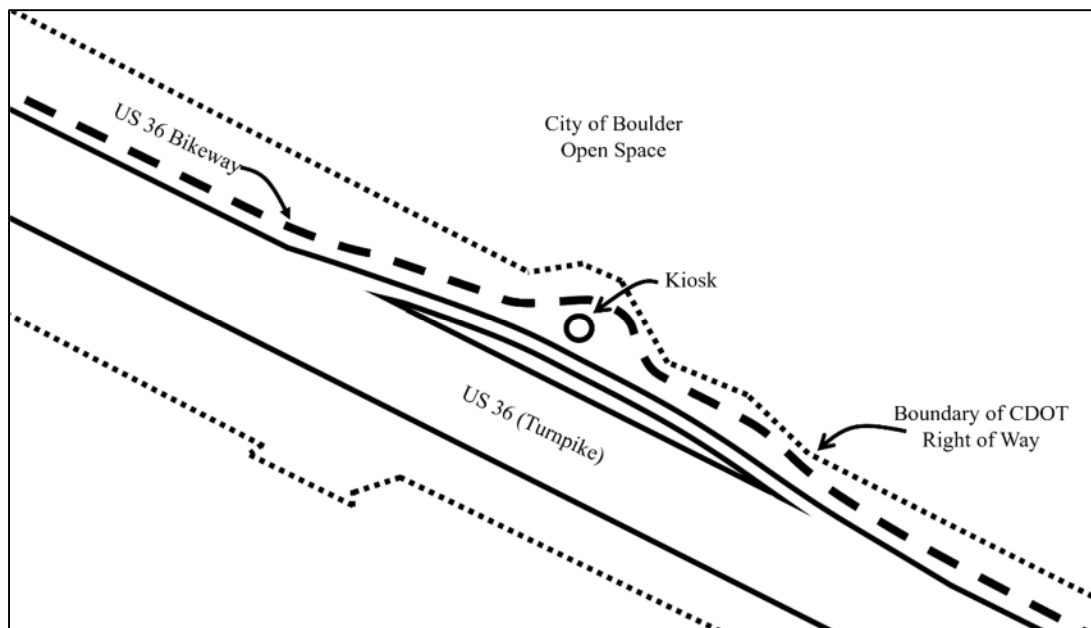
Another commonality is that both the pillars and the memorial at Legion Park have been overlooked for decades. Adding the pillars to the Legion Park site could simply exacerbate the decline of both monuments. Since the futures of the two memorials will be linked, a plan to relocate the pillars to the park should include revitalizing both and aggressively maintaining them in the coming decades.

## Relocation of the Gateway Pillars to US 36 Overlook

Perhaps the most dramatic venue in the East County is the overlook on US 36, aka the Turnpike. The vista from the overlook is spectacular, with the Boulder Valley below and the Continental Divide stretching out in the distance. Relocating the pillars to the overlook would provide them with a superb backdrop at a spot looking out on the zealously protected “frontispiece” or new entrance to Boulder. Moreover, the overlook is a site (a) where the pillars will not again be subject to further movement or disturbance, (b) which is governmentally owned, (c) where efforts could be made to enhance the pillars’ visibility as a memorial, and (d) where both pillars can be located.

### Background

The US 36 overlook is entirely located on the US 36 right of way owned by CDOT. The site is accessible only for westbound traffic from US 36 via a shallow turnout, where there is parking for approximately 8-9 vehicles. In recent years, a bikeway was built adjacent to the westbound lane of US 36, providing access to the overlook for bicyclists and walkers from both Boulder and Louisville.



*Vicinity Map - US 36 Overlook*

At present, the overlook site is small, bracketed by the turnout lane for US 36 and the bikeway. A small kiosk is the primary amenity, along with some interpretive signage, benches, and telescopes. The site has no trees or other formal landscaping, but instead is dryland prairie similar to City of Boulder open space to the north and west.

The overlook’s outstanding feature is the expansive view to the west across the valley to the Rocky Mountains in the distance. Over the past several decades, the City of Boulder spent millions of dollars protecting that view, which is often called Boulder’s “frontispiece.” Functionally, the overlook is the new gateway welcoming visitors from the east to Boulder.



*US 36 Overlook*

### **The Proposal**

The original purpose of erecting the pillars at the intersection of the Lincoln Highway and Arapahoe Road was to create a gateway to a Road of Remembrance leading visitors to Boulder. The Road of Remembrance was never built, and Arapahoe Road has been supplanted by US 36 as the primary route for visitors from the east to Boulder.

By relocating the pillars to the US 36 overlook, they again can be part of a grand entrance to the City of Boulder. The views behind them would be striking, different but comparable to the background for the original pillars. Interpretive signage and other amenities can be added to emphasize and enhance the pillars' memorial purpose.

### **Advantages**

The advantages of the proposed site are apparent:

- The background view behind the pillars on the overlook would be magnificent.
- The pillars would be visible and accessible to westbound visitors headed for Boulder, the intended audience for the original pillars.
- Though additional planning would be needed, the overlook could provide space for signage and other amenities explaining and enhancing the pillars' memorial purpose.
- The overlook provides some public parking, as well as bike access, for those who want to visit the pillars.
- Since the overlook is owned by CDOT, the process of reaching an agreement for the pillars relocation may be less complicated, and CDOT would take responsibility for the perpetual care and maintenance of the pillars and the associated signs and memorials.
- Removing the pillars from the Arapahoe Road – US 287 intersection will permit engineers, if appropriate, to reconfigure that intersection to address safety concerns. While those

decisions are beyond the scope of this project, such a discussion can proceed without the constraints imposed by those historic structures.

### **Disadvantages**

There also are obstacles inherent in the proposal:

- The overlook is situated on US 36, several miles from the “Road of Remembrance” and the pillars’ original location.
- County residents would seldom visit or even see the pillars. Instead of being an integral part of daily life in the East County, the pillars would be primarily a collateral attraction for visitors to the County enjoying the views from the overlook.
- Few passersby stop at the overlook, either from the highway or the bikeway. While the pillars would be visible to westbound traffic on US 36, their location to the side of a high-speed, heavily-trafficked expressway makes it less likely that passers-by would observe them or understand their history and purpose. Bicyclists would be more likely to observe and perhaps stop at the pillars, but their numbers are small compared to the drivers who currently use Arapahoe Road.
- The pillars would not be visible or accessible to eastbound traffic on US 36, forcing all vehicular visitors to approach in the westbound lanes.
- Those stopping at the overlook will need to cope with the noise and activity generated by traffic passing by a few feet away on US 36.
- The overlook is small and there is no obvious place to relocate the pillars given its current configuration. Accordingly, significant sitework may be needed, both to find a place for the pillars and their amenities, and to relocate some of the existing infrastructure. Depending on the site design, it might be necessary to seek a small amount of additional land from the adjacent City of Boulder open space.

### **Discussion**

In some ways, the US 36 overlook is both the best and worst alternative for the pillars. If the goal is to place the pillars in a stunning visual location where visitors can photograph them to great effect, the overlook is an ideal spot. Passersby might seldom appreciate them, yet the pillars could be publicly proclaimed to once again be the “Gateway to Boulder.”

At the same time, the pillars would lose much of their local heritage and become almost exclusively a tourist attraction. Signage and other amenities could be installed to explain their memorial purpose, but the pillars’ location alongside an expressway would make it unlikely that County residents would visit to reflect and honor those who served.

### **Recent Postings on 9milecornerpillars.com**

From: Rebecca Sulka <rsulka@gmail.com>

Date: Tue, Apr 12, 2022 at 8:37 PM

Subject: 9 mile corner Memorial pillars

To: <wdmaddme@gmail.com>

Hello,

Thank you for allowing folks to watch the meeting at the YMCA on 4/12/22 via zoom!  
I understand that one hundred years ago that intersection was intended to be a 'gateway' off the 'Lincoln Highway' to Boulder and the structures are WW1 memorial pillars.

I really appreciated hearing the story of someone who knew they were a memorial and remembers the gateway before the turnpike was built. I've lived just a few miles from them for 28 years and I'm only now learning of their significance. There was no way, even before the recent CDOT work, that I was going to get out of my car and walk around them. Knowing how special the memorial pillars are to that community member, I say this with sympathy, I'm afraid when they built the turnpike, with it's sweeping views of the Boulder valley the 'gateway' purpose of those pillars was replaced by highway 36.

That being said the memorial has not been honored as it should be, like the way that man would straighten up, turn and salute.

I agree with the idea to keep them as close to where they are as possible. My personal hope is that there could be a considerable effort to find somewhere along the "Lincoln" highway where the pillars could stand as a frame of the beautiful back range view, and that one day we could all read the names on those memorials in safety with the opportunity for deep reflection.

That's my 2 cents...

Thanks again,

Becky Sulka

303-907-8025

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From: Steve Hartmeyer <sjh17@indra.com>

Date: Thu, Apr 14, 2022 at 9:17 PM

Subject: Pillar comments post-forum

To: <wdmaddme@gmail.com>

To the Members of the Mitigation Working Group:

I'm offering my commentary as a 24-year resident of Lafayette with an interest in history, local and otherwise, as the son of a U.S. Army veteran (of Vietnam), and as a citizen who has passed by the pillars on errands or my commute nearly every day for these last 24 years. I attended the public forum at the YMCA in Lafayette out of concern for the future of the pillars, and was very gratified to learn via that forum that there are many individuals and groups that have taken an interest in trying to resolve the present situation favorably.

I would like to express my appreciation to Bill Meyer and the Boulder Rotary Club in particular for spearheading efforts to communicate with the general public about this matter, and to CDOT for soliciting community involvement in determining a course of action.

In hearing numerous constructive comments from my fellow citizens during the forum, and seeing the written alternatives offered in the pamphlet presented at the forum, my original generalized opinion of the situation has become clarified, and I would share it thusly:

First of all, it appears nearly self-evident that although preservation in-situ might be desirable to many for the historicity of the location, and potentially less expensive, in-situ "preservation" cannot possibly be considered practical.

A reconstruction of the intersection of US 287 and Arapahoe, impactful to the pillars, has just been completed. Relocation of one of the pillars previously took place in an earlier reconstruction of this intersection. There is every reason to expect that this intersection must be reconstructed again in the future due to continuing growth and changing transportation needs in the vicinity. There can and will only be more traffic at this location in the future as Erie continues to expand and the population of Boulder County grows. Furthermore, several speakers at the forum very clearly complained about the current configuration of the intersection with apparent justification, suggesting that there will be considerable impetus to have the intersection subject to reconfiguration again immediately or in the very near future, and any further adjustments are likely to be a detriment to the pillars.

Therefore, I argue there is no valid choice except to relocate the pillars, despite the potential expense.

I was pleased to see a list of suggested alternative locations expressed in the pamphlet, several of which were described in remarks by Mr. Meyer. I concur with the opinion expressed by the representative of the Daughters of the American Revolution who attended, that the pillars if relocated should be placed in a space that will permit visitors to view them close-up, and that such a space should include interpretive signage, at least similar in quality to interpretive signage common in Boulder County Open Space.

It should be obvious that such signage should include information about the pillars' former location, and the reasons for being placed there originally, and the reasons for relocation.

I very much appreciated Mr. Meyer's suggestion that members of the CU ROTC cadre attend the pillars on Veteran's Day. Although for various reasons locating the pillars on the CU campus may not be ideal, I should like to think that a relationship with the CU ROTC program could and should be fostered such that the pillars may be treated as the memorial they were meant to be, and become a site for remembrance ceremonies. When I was a Boy Scout, I recall that ceremonies of respect for veterans were occasionally attended by members of my troop, and perhaps local Girl Scout and Boy Scout troops could similarly be enlisted along with ROTC students and various local veterans' groups to volunteer to help maintain the grounds and organize appropriate ceremonies.

Several locations mentioned in the pamphlet are appealing, foremost among them in my mind possibly being Legion Park. I am not very familiar with this park except to know its location along Arapahoe Road, despite having lived in Boulder County for 29 years, but it seems to have the potential to be a quiet space conducive to remembrance and ceremony. Many other suggested locations in busier parts of the county would lack this advantage (for example, the US 36 overlook may be scenic, and a gateway, but I think constant noise from an immediately-adjacent major divided highway is hardly appropriate for peace and remembrance).

As much as I would like to recommend keeping the pillars as near as possible to their original location (now in Lafayette), I think it is most important to place them in a new location where (a) they can be decently preserved, (b) they can be readily visited by the public, (c) they can be accompanied by signage, and ideally (d) new memorials to veterans of more recent conflicts (WWII, Korea, Vietnam, Iraq, and Afghanistan were all mentioned in passing at the forum) may be added to a potential landscape or garden of remembrance.

It would seem that an existing large park or open space location would lend itself to these goals better than placement near retail or within a confined municipal public space might do.

Thank you for the opportunity to share my opinion. When it comes time to gather funding for the expenses to removing and preserving the pillars, please ensure to contact me. I pledge at this time to contribute no less than \$1000.00 toward this cause once a funding vehicle is available.

Best regards,

Steven J. Hartmeyer

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From: Barbara Torres <BTorres@care4denver.org>

Date: Tue, Apr 26, 2022, 10:06 AM

Subject: Pillar preservation

To: wdmaddme@gmail.com <wdmaddme@gmail.com>

The purpose of the pillars was not only to honor the soldiers of WWI but also to entice tourists to come to Boulder. It's important to keep in mind this DUAL purpose. The pillars were meant to be a Gateway and were put at their location for a reason. They were not meant for people to get out of their cars and have a moment/place to reflect. All of Arapahoe was the "Road of Remembrance" where people could reflect about the soldiers of Boulder County as they drove to Boulder.

The pillars aren't in anyone's way. Let's not rape the intersection like Boulder did with the 75th Street intersection.

The people who want the pillars moved are likely not Boulder County natives. They didn't drive through them to get to Denver like we used to do. They have no special ties or memories of the pillars. To move them would render them almost meaningless because they would lose half of their purpose. They would no longer be a Gateway to Boulder from 287 they were intended to be.

Barbara (Woodley) Torres

Boulder native